

# Development Control B Committee Agenda



**Date:** Wednesday, 9 June 2021

**Time:** 2.00 pm

**Venue:** The Council Chamber - City Hall, College Green, Bristol, BS1 5TR

## **Distribution:**

**Councillors:** Ani Stafford-Townsend (Chair), Chris Windows (Vice-Chair), Donald Alexander, Lesley Alexander, Fabian Breckels, Andrew Brown, Tony Dyer, Zoe Goodman and Guy Poultney

**Copies to:** Gary Collins, Laurence Fallon, Matthew Cockburn, Zoe Willcox and Jeremy Livitt

**Issued by:** Jeremy Livitt, Democratic Services

City Hall, PO Box 3167, Bristol, BS3 9FS

E-mail: [democratic.services@bristol.gov.uk](mailto:democratic.services@bristol.gov.uk)

**Date:** Tuesday, 1 June 2021



[www.bristol.gov.uk](http://www.bristol.gov.uk)

# Agenda

## **1. Welcome, Introduction and Safety Information**

**2.00 pm**

**(Pages 6 - 9)**

## **2. Confirmation of Chair for the 2021/22 Municipal Year**

The Committee is requested to note the appointment at the Annual Full Council meeting on Tuesday 25<sup>th</sup> May 2021 of Councillor Ani Stafford-Townsend as Chair of the Development Control B Committee for 2021/22 Municipal Year.

## **3. Confirmation of Vice-Chair for the 2021/22 Municipal Year**

The Committee is requested to note the appointment at the Annual Full Council Meeting on Tuesday 25<sup>th</sup> May 2021 of Councillor Chris Windows as Vice-Chair of the Development Control B Committee for the 2021/22 Municipal Year

## **4. Membership of the Development Control B Committee - 2021/22 Municipal Year**

The Committee is requested to note the appointment of the following Councillors for Development Control B Committee for the 2021/22 Municipal Year:

**Councillor Ani Stafford-Townsend (Chair)**

**Councillor Chris Windows (Vice-Chair)**

**Councillor Fabian Breckels (Labour Party Group Spokesperson)**

**Councillor Andrew Brown (Liberal Democrat Group Spokesperson)**

**Councillor Donald Alexander**

**Councillor Lesley Alexander**

**Councillor Tony Dyer**

**Councillor Zoe Goodman**

**Councillor Guy Poultney**

## **5. Terms of Reference**

The Committee is requested to note the Terms of Reference as agreed at the Annual Meeting of Full Council on Tuesday 25<sup>th</sup> May 2021.

**(Pages 10 - 13)**



## **6. Dates of Future Meetings - 2021/22 Municipal Year**

The following dates are proposed for meetings of the Development Control B Committee for the 2021/22 Municipal Year:

**6pm on Wednesday 21<sup>st</sup> July 2021**

**2pm on Wednesday 1<sup>st</sup> September 2021**

**6pm on Wednesday 13<sup>th</sup> October 2021**

**2pm on Wednesday 24<sup>th</sup> November 2021**

**6pm on Wednesday 12<sup>th</sup> January 2022**

**2pm on Wednesday 23<sup>rd</sup> February 2022**

**6pm on Wednesday 6<sup>th</sup> April 2022**

**2pm on Wednesday 18<sup>th</sup> May 2022**

## **7. Apologies for Absence and Substitutions**

The Committee is requested to note apologies for absence and substitutions for the meeting which will be reported.

## **8. Declarations of Interest**

To note any interests relevant to the consideration of items on the agenda.

Any declarations of interest made at the meeting which are not on the register of interests should be notified to the Monitoring Officer for inclusion.

## **9. Minutes of the previous meeting held on Thursday 15th April 2021**

To agree the minutes of the last meeting held on Thursday 15<sup>th</sup> April 2021 as a correct record.

**(Pages 14 - 19)**

## **10. Appeals**

To note appeals lodged, imminent public inquiries and appeals awaiting decision.

**(Pages 20 - 30)**

## **11. Enforcement**

To note enforcement notices.

**(Page 31)**



## 12. Public Forum

All attendees for the meeting are requested to note that, due to Covid Safety requirements we have put the following measures in place:

- All attendees to this meeting are asked to have a Covid lateral flow test 24 hrs prior to the day of the meeting and show the results of a negative test. It's important that you report the results of your test and that you get confirmation sent to your phone. Reception staff will ask to see this on the day of the meeting. If you have a positive test or if you develop any Covid 19 symptoms - high temperature, a new continuous cough, or a loss or change to your sense of smell or taste, you should book a test on GOV.UK and self-isolate while you wait for the results.
- You are required to wear a face mask at all times unless you are exempt. Social distancing rules remain in place.
- Members of the press and public who wish to attend City Hall are advised that you will be asked to watch the meeting on a screen in another room as due to the maximum occupancy of the venue.

Any member of the public or councillor may participate in public forum. The detailed arrangements for so doing are set out in the Public Information Sheet at the back of this agenda. Please note that the following deadlines will apply in relation to this meeting:

### Questions:

Written questions must be received three clear working days prior to the meeting. For this meeting, this means that your question(s) must be received at the latest **by 5pm on Thursday 3<sup>rd</sup> June 2021.**

### Petitions and statements:

Petitions and statements must be received by noon on the working day prior to the meeting. For this meeting, this means that your submission must be received at the latest **by 12 Noon on Tuesday 8<sup>th</sup> June 2021.**

The statement should be addressed to the Service Director, Legal Services, c/o The Democratic Services Team, City Hall, 3<sup>rd</sup> Floor Deanery Wing, College Green, P O Box 3176, Bristol, BS3 9FS or email - [democratic.services@bristol.gov.uk](mailto:democratic.services@bristol.gov.uk)

Members of the public who wish to present their public forum statement, question or petition at the zoom meeting must register their interest by giving at least two clear working days' notice prior to the meeting by **2pm on Monday 7<sup>th</sup> June 2021.**

**PLEASE NOTE THAT IN ACCORDANCE WITH THE NEW STANDING ORDERS AGREED BY BRISTOL CITY COUNCIL, YOU MUST SUBMIT EITHER A STATEMENT, PETITION OR QUESTION TO ACCOMPANY YOUR REGISTER TO SPEAK.**





In accordance with previous practice adopted for people wishing to speak at Development Control Committees, please note that you may only be allowed 1 minute subject to the number of requests received for the meeting.

**Please also note that this meeting is being held as a physical meeting to ensure compliance with the relevant Government legislation concerning decisions made by Development Control Committee meetings.**

**Therefore, if you wish to speak to a Public Forum Statement, you will need to attend the meeting at City Hall, College Green, Bristol. However, all members of the public will be able to submit Public Forum Statements as normal and watch the meeting as it is being webcast by clicking on the appropriate link on the relevant webpage of the Bristol City Council website.**

### **13. Planning and Development**

To consider the following applications for Development Control Committee B - **(Page 32)**

- a) Planning Application Number 20/06030/F - 7 Belvedere Road (Pages 33 - 67)**
- b) Planning Application Number 20/04678/F - St Johns Lane Health Centre (Pages 68 - 98)**

### **14. Date of Next Meeting**

Subject to agreement of future dates for the remainder of the 2021/22 Municipal Year (Agenda Item 6), the next meeting of the Development Control B Committee is scheduled to be held at 6pm on Wednesday 21<sup>st</sup> July 2021.



# Public Information Sheet

## Inspection of Papers - Local Government (Access to Information) Act 1985

You can find papers for all our meetings on our website at [www.bristol.gov.uk](http://www.bristol.gov.uk).

## Covid-19: changes to how we hold public meetings

Following changes to government rules, public meetings including Cabinet, Full Council, regulatory meetings (where planning and licensing decisions are made) and scrutiny will now be held at City Hall.

## Covid Safety Measures for Attendance at City Hall

Due to Covid Safety requirements we have put the following measures in place:

- All attendees to this meeting are asked to have a Covid lateral flow test 24 hrs prior to the day of the meeting and show the results of a negative test. It's important that you report the results of your test and that you get confirmation sent to your phone. Reception staff may ask to see this on the day of the meeting. If you have a positive test or if you develop any Covid 19 symptoms - high temperature, a new continuous cough, or a loss or change to your sense of smell or taste, you should [book a test on GOV.UK](https://www.gov.uk/book-a-test) and self-isolate while you wait for the results.
- You are required to wear a face mask at all times unless you are exempt. Social distancing rules remain in place.
- Members of the press and public who wish to attend City Hall are advised that you may be asked to watch the meeting on a screen in another room as due to the maximum occupancy of the venue.

## Other formats and languages and assistance for those with hearing impairment

You can get committee papers in other formats (e.g. large print, audio tape, braille etc) or in community languages by contacting the Democratic Services Officer. Please give as much notice as possible. We cannot guarantee re-formatting or translation of papers before the date of a particular meeting.

Committee rooms are fitted with induction loops to assist people with hearing impairment. If you require any assistance with this please speak to the Democratic Services Officer.

## Public Forum

Members of the public may make a written statement ask a question or present a petition to most meetings. Your statement or question will be sent to the Committee Members and will be published on the Council's website before the meeting. Please send it to [democratic.services@bristol.gov.uk](mailto:democratic.services@bristol.gov.uk).



The following requirements apply:

- The statement is received no later than **12.00 noon on the working day before the meeting** and is about a matter which is the responsibility of the committee concerned.
- The question is received no later than **5pm three clear working days before the meeting**.

Any statement submitted should be no longer than one side of A4 paper. If the statement is longer than this, then for reasons of cost, it may be that only the first sheet will be copied and made available at the meeting. For copyright reasons, we are unable to reproduce or publish newspaper or magazine articles that may be attached to statements.

By participating in public forum business, we will assume that you have consented to your name and the details of your submission being recorded and circulated to the Committee and published within the minutes. Your statement or question will also be made available to the public via publication on the Council's website and may be provided upon request in response to Freedom of Information Act requests in the future.

We will try to remove personal and identifiable information. However, because of time constraints we cannot guarantee this, and you may therefore wish to consider if your statement contains information that you would prefer not to be in the public domain. Other committee papers may be placed on the council's website and information within them may be searchable on the internet.

#### During the meeting:

- Public Forum is normally one of the first items on the agenda, although statements and petitions that relate to specific items on the agenda may be taken just before the item concerned.
- There will be no debate on statements or petitions.
- The Chair will call each submission in turn. When you are invited to speak, please make sure that your presentation focuses on the key issues that you would like Members to consider. This will have the greatest impact.
- Your time allocation may have to be strictly limited if there are a lot of submissions. **This may be as short as one minute.**
- If there are a large number of submissions on one matter a representative may be requested to speak on the groups behalf.
- If you do not attend or speak at the meeting at which your public forum submission is being taken your statement will be noted by Members.
- Under our security arrangements, please note that members of the public (and bags) may be searched. This may apply in the interests of helping to ensure a safe meeting environment for all attending.
- As part of the drive to reduce single-use plastics in council-owned buildings, please bring your own water bottle in order to fill up from the water dispenser.

For further information about procedure rules please refer to our Constitution

<https://www.bristol.gov.uk/how-council-decisions-are-made/constitution>



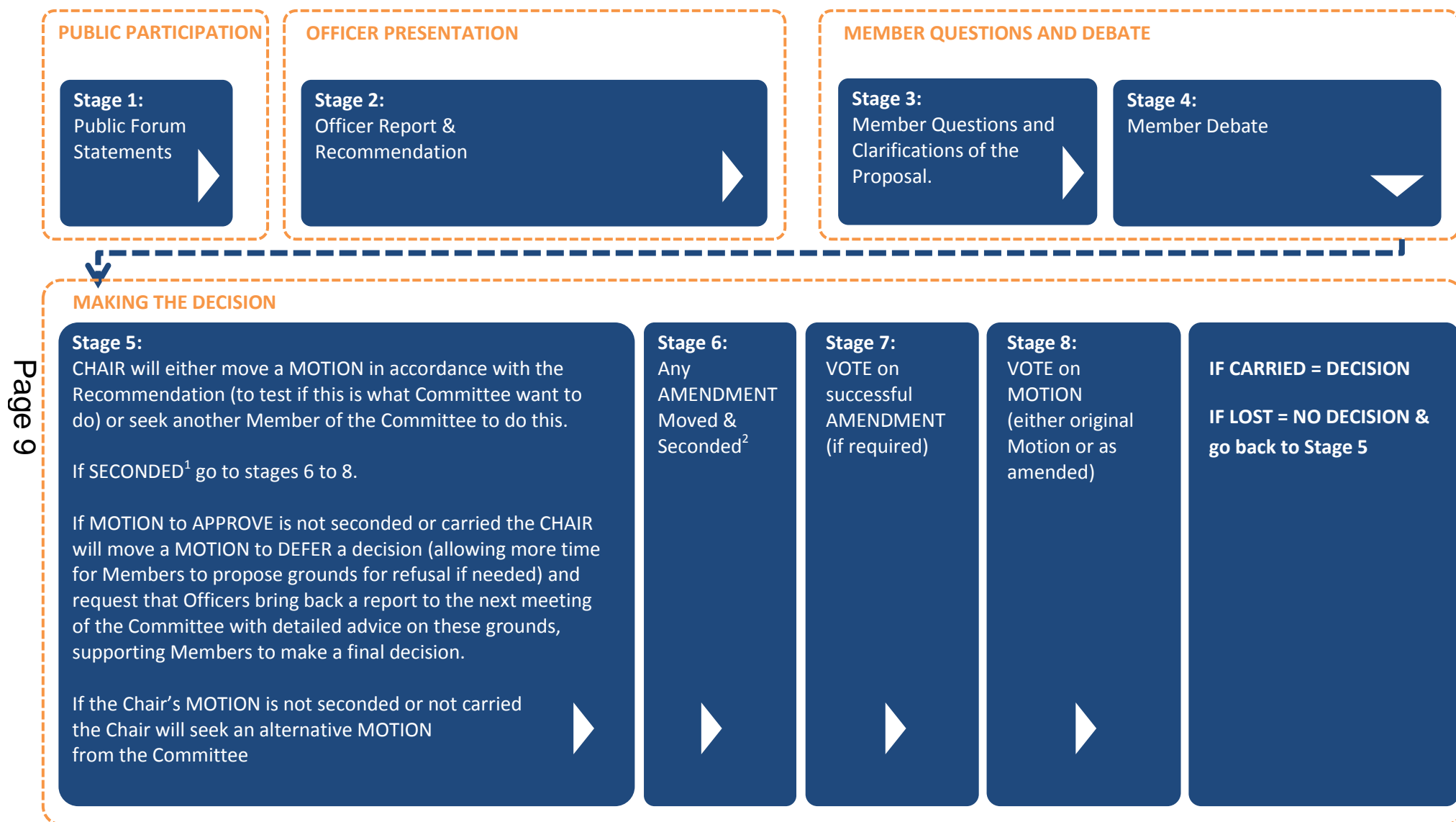
## Webcasting/ Recording of meetings

Members of the public attending meetings or taking part in Public forum are advised that all Full Council and Cabinet meetings and some other committee meetings are now filmed for live or subsequent broadcast via the council's [webcasting pages](#). The whole of the meeting is filmed (except where there are confidential or exempt items). If you ask a question or make a representation, then you are likely to be filmed and will be deemed to have given your consent to this. If you do not wish to be filmed you need to make yourself known to the webcasting staff. However, the Openness of Local Government Bodies Regulations 2014 now means that persons attending meetings may take photographs, film and audio record the proceedings and report on the meeting (Oral commentary is not permitted during the meeting as it would be disruptive). Members of the public should therefore be aware that they may be filmed by others attending and that is not within the council's control.

The privacy notice for Democratic Services can be viewed at [www.bristol.gov.uk/about-our-website/privacy-and-processing-notices-for-resource-services](http://www.bristol.gov.uk/about-our-website/privacy-and-processing-notices-for-resource-services)



# Development Control Committee Debate and Decision Process



<sup>1</sup> A Motion must be Seconded in order to be formally accepted. If a Motion is not Seconded, the debate continues

<sup>2</sup> An Amendment can occur on any formally approved Motion (ie. one that has been Seconded) prior to Voting. An Amendment must itself be Seconded to be valid and cannot have the effect of negating the original Motion. If Vote carried at Stage 7, then this becomes the Motion which is voted on at Stage 8



## **DEVELOPMENT CONTROL COMMITTEES**

### **Terms of Reference**

#### **Arrangements**

There are two Development Control Committees:

- Development Control Committee “A”
- Development Control Committee “B”

Each Development Control Committee shall have full authority to deal with all development control matters reserved to a Development Control Committee by virtue of this consultation.

#### **Functions**

Full Council has delegated to the Development Control Committees all functions relating to town and country planning and development control as specified in Regulation 2 and Schedule 1 to the Local Authorities (Functions and Responsibilities) (England) Regulations 2000 (as amended) under the following provisions and any related secondary legislation:

1. Power to determine applications for planning permission (section 70(1) (a) and (b) and 72 of the Town and Country Planning Act 1990 (c.8)).
2. Power to determine applications to develop land without compliance with conditions previously attached (section 73 of the Town and Country Planning Act 1990).
3. Power to grant planning permission for development already carried out (section 73(A) of the Town and Country Planning Act 1990).
4. Power to decline to determine application for planning permission (section 70A of the Town and Country Planning Act 1990).
5. Duties relating to the making of determinations of planning applications (Sections 69, 76 and 92) of the Town and Country Planning Act 1990 and Articles 8, 10 to 13, 15 to 22 and 25 and 26 of the Town and Country Planning (General Development Procedure Order 1995) (S.I. 1995/419 and directions made thereunder).
6. Power to determine application for planning permission made by a local authority, alone, or jointly with another person (section 316 of the Town and Country Planning General Regulations 1992 (S.I. 1992/1492)).
7. Power to make determinations, give approvals and agree certain other matters relating to the exercise of permitted development rights (Parts 6, 7, 11, 17, 19, 20, 21 to 24, 26, 30 and 31 of Schedule 2 to the Town and Country Planning (General Permitted Development) Order 1995).
8. Power to enter into agreement regulating development or use of land (Section 106 of the Town and Country Planning Act 1990).

9. Power to issue a certificate of existing or proposed lawful use or development (Section 191(4) and 192(2) of the Town and Country Planning Act 1990).
10. Power to serve a completion notice (Section 94(2) of the Town and Country Planning Act 1990).
11. Power to grant consent for the display of advertisements (Section 220 of the Town and Country Planning (Control of Advertisements) Regulations 1992).
12. Power to authorize entry onto land (Section 196A of the Town and Country Planning Act 1990).
13. Power to require the discontinuance of a use of land (Section 102 of the Town and Country Planning Act 1990).
14. Power to serve a planning contravention notice, breach of condition notice or stop notice (Sections 171C, 187A and 183(1) of the Town and Country Planning Act 1990).
15. Power to issue a temporary stop notice (Section 171E of the Town and Country Planning Act 1990).
16. Power to issue an enforcement notice (Section 172 of the Town and Country Planning Act 1990).
17. Power to apply for an injunction restraining a breach of planning control (Section 18 7B of the Town and Country Planning Act 1990).
18. Power to determine applications for hazardous substances consent and related powers (Sections 9(1) and 10 of the Planning (Hazardous Substances) Act 1990 (c.10)).
19. Duty to determine conditions of which old mining permissions, relevant planning permissions relating to dormant sites or active Phase I or II sites or mineral permissions relating to mining sites, as the case may be, are to be subject (paragraph 2(6) (a) of Schedule 2 of the Planning and Compensation Act 1991, paragraph 9(6) of the Schedule 13 of the Environment Act 1995 (c.25) and paragraph 6(5) of Schedule 14 to that Act).
20. Power to require proper maintenance of land (section 215(1) of the Town and Country Planning Act 1990).
21. Power to determine application for listed building consent and related powers (sections 16(1) and (2), 17 and 33(1) of the Planning (Listed Buildings and Buildings in Conservation Areas) Act 1990 (c.9).
22. Power to issue Certificate of Lawful Works to listed buildings The Planning (Listed Buildings and Conservation Areas) Act 1990: sections 26H and 26I, as inserted by section 61 of the Enterprise and Regulatory Reform Act 2013. The Planning (Listed Buildings) (Certificates of Lawfulness of Proposed Works) Regulations 2014

23. Duties relating to applications for listed building consent and Certificates of Lawful Works and conservation area consent (sections 13(1) and 14(1) and (4) of the Planning (Listed Buildings and Conservation Areas) Act 1990 and regs 3 to 6 and 13 of the Planning (Listed Buildings and Conservation Areas) Regulations 1990 S.I. 1990/1519) and paragraphs 8, 15 and 26 of the Department of Environmental, Transport and the Regions circular 01/01).
24. Power to serve a building preservation notice and related powers (sections 3(1) and 4(1) of the Planning (Listed Buildings and Buildings in Conservation Areas) Act 1990).
25. Power to issue enforcement notice in relation to demolition of listed building in conservation area (section 38 of the Planning (Listed Buildings and Conservation Areas) Act 1990).
26. Powers to acquire a listed building in need of repair and to serve a repairs notice (section 47 and 48 of the Planning (Listed Buildings and Conservation Areas) Act 1990).
27. Power to apply for an injunction in relation to a listed building (section 44A of the Planning (Listed Buildings and Conservation Areas) Act 1990).
28. Power to execute urgent works to unoccupied listed buildings (section 54 of the Planning (Listed Buildings and Conservation Areas) Act 1990).
29. Power to authorize stopping up or diversion of highway (section 247 of the Town and Country Planning Act 1990).
30. Power to authorise stopping-up or diversion of footpath, bridleway or restricted byway (section 257 of the Town and Country Planning Act 1990).
31. Power to extinguish public rights of way over land held for planning purposes (Section 258 of the Town and Country Planning Act 1990).
32. Powers relating to the protection of important hedgerows (the Hedgerows Regulations 1997 (S.I. 1997/1160)).
33. Powers relating to the preservation of trees (sections 197 to 214D of the Town and Country Planning Act 1990 and the Trees Regulations 1990 (S.I. 1999/1892)).
34. Powers relating to complaints about high hedges (Part 8 of the Anti-Social Behaviour Act 2003).
35. Power to include modifications in other orders (Section 53A of the Wildlife and Countryside Act 1981).
36. Power to revoke or modify planning permission (Section 97 Town and Country Planning Act 1990).
37. Duty to enter land in Part 2 of the brownfield land register (Regulation 3 of the Town and Country Planning (Brownfield Land Register) Regulations 2017).



38. The making of Local Development Orders under Article 5 of the Town and Country Planning (General Development Procedure) (Amendment) (England) Order 2006.

39. Power to exercise functions relating to Nationally Significant Infrastructure Projects and Development Consent Orders as contained in the Planning Act 2008.

**Code of Conduct**

The committee must follow the Council's Code of Conduct for Councillors and Officers dealing with development control and other appropriate planning matters to be considered by Full Council at its meeting on Thursday 21<sup>st</sup> May 2020.

## Bristol City Council Minutes of the Development Control B Committee

15 April 2021 at 6.00 pm



### **Members Present:-**

**Councillors:** Tom Brook (Chair), Richard Eddy (Vice-Chair), Lesley Alexander, Fi Hance, Chris Jackson, Olly Mead, Nicola Bowden-Jones and Sultan Khan

### **Officers in Attendance:-**

Gary Collins, Laurence Fallon, Matthew Cockburn, Paul Chick and David MacFadyen

#### **1. Welcome, Introduction and Safety Information**

All parties were welcomed to the meeting.

#### **2. Apologies for Absence**

Cllr Stephen Clarke sent apologies

Cllr Mike Davies sent his apologies, Cllr Fabian Breckels is substituting.

#### **3. Declarations of Interest**

None received.

#### **4. Minutes of the previous meeting held on 17th March 2021**

RESOLVED the minutes of the previous meeting on 17 March 2021 were agreed as a correct record.

#### **5. Appeals**

The Head of Development Management introduced the report bringing the following to Committee's attention:

- Item 10 on p.13 relates to the former police dog and horse training ground. Committee granted an application to move Baltic Wharf Caravan Club to that location. Due to potential flooding issues, the application had to be referred to the Secretary of State, who has called it in. There will be an enquiry from 20 July 2021, and we will need to field witnesses to defend that decision.

**ACTION GC to send an update email to RE about item 57 (Dancey Mead).**



## 6. Enforcement

The Head of Development Management reported that 3 new notices had been served since the last meeting. There were no questions from committee.

## 7. Practice Notes - Information Item

This report was NOTED.

There was agreement at a recent Development Control leads meeting to add practice notes as standing items on DC meetings to make sure all members are up to speed on the relevant legislation. This particular update relates to space standards and climate change and sustainability.

## 8. Public Forum

Members of the Committee received Public Forum Statements in advance of the meeting. The Statements were heard before the application they related to and were taken fully into consideration by the Committee prior to reaching a decision.

## 9. Planning and Development

The Committee considered the following Planning Applications.

### 9.a. 20/04474/F We The Curious, Millennium Square

An Amendment Sheet was provided to the Committee in advance of the meeting, detailing changes since the publication of the original report.

The Planning Officer summarised the application as follows:

- a. The proposed location contains lots of existing attractions and listed buildings, including the We the Curious science museum, cathedral, cathedral school and central library.
- b. The committee was shown the existing elevation of the We the Curious building alongside the Planetarium, and then with the addition of the Arc. The capsule rests on the roof of the WtC building. Total height of the Arc is 79m. The cabin floor is 67m from ground level. Base is 3m diameter. 42 people are carried in the capsule for 20-minute session including boarding and offloading. There will be a guide on board to describe points of interest. It will operate for a maximum of 18 hours per day. It will be in the air for 60% of the run time and be at its highest point for half of that. It is expected to draw around 250k visitors per year.
- c. Some views have been raised from the Bristol Walking Alliance and the Bristol Civic Society about the impact of the Arc base on traffic flow and the fulcrum going overhead distracting people in Millennium Square. It could inhibit use of the fountains, pools and there may be insufficient



space for cyclists and pedestrians in the pinch point between Millennium Square and Anchor Square.

- d. The Arc will be visible from College Green over the Cathedral, impacting on the skyline. It would also appear above the Cathedral School and the historic gatehouse. It will be visible from most parts of the harbourside. Planning within the harbourside has a general principle of keeping views of the cathedral unobstructed.
- e. There have been 35 responses. 32 in support and 3 against. Points in support are tourism, economy, employment, raising Bristol's profile and education. Points against are the impact on the square, harm to the skyline, bulky design, exclusivity of use and similar views being available elsewhere.
- f. There were mixed views on the design. In itself, it is a high-quality design but very different to the existing context. Therefore, there is not a conclusive stance on design. There is harm to heritage assets, but this is classed as 'less than substantial harm' so should be weighed against the public benefit. It is the view of Historic England that the public benefit does not mitigate harm to the existing heritage assets.
- g. In terms of education, there will be a guide on the Arc talking about the history of Bristol. 10% of profits would go to a social innovation programme to improve access to the Arc for people from disadvantaged backgrounds. This would work out to 1% free tickets of the projected 250k annual total. The economic benefit will be significant. National policy is that great weight must be given to heritage asset protection, in this case the Cathedral, other grade II\* and grade II listed buildings, and harm to the character and appearance of 3 conservation areas (College Green, City and Docks and Queen Square).

#### Questions for Clarification:

- h. When Millennium Square was being proposed, what weight was given to heritage assets then? MS is now a mix of old and new, with the planetarium, fountains, aquarium and WtC all in modern style. Officers accept there have been a lot of changes to the area. WtC received awards for the roof extension as it was sensitive to the style of the existing building.
- a. There was a discussion about the new University Library, which had similar arguments about heritage assets. The library does have an impact on heritage, but the public benefit outweighs this. Heritage assets in this case are more significant, Bristol Grammar School versus Bristol Cathedral. Heritage harm vs public benefit is a subjective decision and committee will have their own view. Members should not make comparisons with the library.
- b. Conditions have not been agreed yet, officers would have to wait for the outcome of this committee. If approved, committee would delegate to officers to agree the conditions, including s.106 funds.
- c. The only issue raised by BCC legal is the percentage profit to charity and the suitable vehicle for that (legal agreement or planning condition). 3000 social tickets per annum is only an estimate, we cannot be certain how many people this will attract. WtC already has an outreach programme so the Arc would likely be an extension of that. It is difficult to predict tourist numbers over the next couple of years considering Covid. If the Arc is unsuccessful it can be removed relatively easily.



- d. There have been some questions about potential noise, however Pollution Control officers have determined this will not be significant.

#### Discussion Notes:

- e. RE commented that members and officers will sometimes disagree on applications and this is one such case. Modern stylish structures can complement the existing buildings. RE intends to move for approval subject to officer conditions being agreed.
- f. FB agreed with RE and believed the importance of the view to the cathedral from the harbour was being overstated. The harbour has several modern structures such as the serpentine bridge which complements the area. The Dean of the Cathedral supports this application, which is significant. Bristol will need economic boosts when coming out of Covid restrictions. Would ask officers to consider out of sight storage at night for the Arc as a condition.
- g. FH thought this was a challenging application for officers but intends to approve. There have been applications with considerably bigger impact on the skyline approved recently. FH was not convinced the economic arguments were as strong as presented.
- h. LA intended to approve the application, saying Bristol was often behind the curve in respect of modern developments.
- i. OM said he was unlikely to use it personally but recognised the appeal it would have. It is important to have tourist attractions in central Bristol, but he agreed with FH that it would not be as popular as projected. OM was encouraged by the support of the Cathedral Trust and that the Arc can be removed if it is not successful.
- j. CJ was interested in the public statements, with many positive submissions from leaders in the area, and one negative statement about noise, which officers have refuted. CJ was convinced the benefit outweighs harm.
- k. SK thanked officers for their detailed work on this application. He also thought benefit outweighed harm and was reassured by the community support, including the cathedral. He thought it would be an asset for the future and complemented existing assets.

No Councillor moved to vote to refuse the application as per the officers' recommendation.

Cllr Richard Eddy proposed, seconded by Cllr Fabian Breckels, to approve the application based on the public benefits of heritage, design, education and economy outweighing the harm to heritage assets, and to delegate conditions to officers.

**RESOLVED: (8 For / 0 Against / 0 Abstain) that the application be approved and delegated to officers.**

#### **9.b. 19/03660/F Astry Close, Lawrence Weston**

The Planning Officer summarised the application as follows:

- a. There is a lot to commend the application, officers regret that they are recommending refusal. There has been a lot of effort to negotiate to make this workable, but there has been no attempt to compromise by the developer over the past 18 months.



- b. Local people are in support and have a valid view on the application, but so does the planning authority. When a planning officer goes through scheme, they look at quality of design, what would it provide, would it realistically work?
- c. In 2012 the site had 16 houses in 8 pairs in precast concrete, which have been demolished and cleared. The site has been vacant for a long period. It is a challenging site due to the steep gradient.
- d. 36 dwellings are proposed, with 32 houses and 4 flats. All are classed as affordable. There is a good mix of semi-detached, terrace and flats. Construction is grey brick. All properties have private gardens. There are 48 parking spaces and 23 trees. The properties are supplied with air source heat pumps and solar panels. There is a substantial split level on the site with a large retaining wall. A public realm "heart space" is proposed between two rows of terraces in the centre of the development.
- e. There are 19 support statements, including the ward councillors. The Council's City Design and Landscape teams have objected. The estate has green verges to provide an attractive open and airy environment. The local plan says they should be retained. DM17 policy is not to develop an open space with visual amenity. This application will be building on a verge, which is a major reason for referral to committee. The Development of 5 terraced properties on Goodring Hill would be beyond the existing building lines.
- f. Houses in the middle and far end of the site have 90-degree angles causing proximity issues, the change in height also compounds this. There is 4m height difference between houses. The spacing between facing windows is only 11m. Recommendations are 18m spacing, with some leeway for dense inner-city areas, but this cannot be justified in a suburban location. The short distance between properties will impact on residents.
- g. The gardens are small as is the proposed 'heart space'. The terraces in the heart space face each other with a 9m spacing. This is not a high-quality shared space. It includes steps, which present access issues. Public use of the space is likely to be a nuisance to residents in the central terraces.
- h. Officers recommend refusal. We accept there are positives and commend the community engagement, but this development falls short of several fundamental planning principles.

#### Questions for Clarification:

- i. The area plan is a statutory document. The dispute is between BCC and the development forum. Community led engagement should be given weight, but committee should also be mindful of the policies and the creation of a quality living environment. We have granted high density developments in the past but not something with this level of overlooking and lack of shared space. It is up to the committee to balance arguments to see if there is enough benefit for the community to outweigh officer concerns. Any policy in the statutory development plan should be given primacy, but officers understand this is a difficult balance.
- j. The view of local community is that the verges are not being used positively even if the plan deems them attractive. The plan was also developed with the local community.
- k. There is a major oak tree on the development which members would seek to retain as a condition should the application be approved.

#### Discussion Notes:

- l. FH felt conflicted. The people who have spoken in favour of this are very high integrity. The design is not ideal and has some significant drawbacks. There are complaints about council policy being too restrictive.
- m. RE intended to support the application. He understood it is a hard balancing act, but there is a



significant amount of public benefit from the development and it has support from the local community and ward councillors.

- n. CJ agreed that the application was a good example of working with the local community and that the benefit outweighs the harm.
- o. FB intended to support the application, acknowledging the large amount of work that officers had put into the case. The heart space could have been bigger and the other verge used for a playground. If residents find the verges to be a nuisance, we should listen to them. The houses themselves are designed well. People in the houses facing the heart space will know what that implies in terms of noise.
- p. OM thought the local plan was positive and forward looking. Other more objectionable schemes have been approved at this committee. The design of houses is good, there are some access issues. He agreed the oak tree should be protected.
- q. SK intended to support the application based on the local community understanding their own needs.

No Councillor moved to vote to refuse the application as per the officers' recommendation.

Cllr Richard Eddy proposed, seconded by Cllr Chris Jackson, to approve the application and to delegate conditions to officers. One of these conditions should be the protection of the oak tree.

**RESOLVED: (8 For / 0 Against / 0 Abstain) that the application be approved and delegated to officers.**

The meeting ended at 8.45 pm

**CHAIR** \_\_\_\_\_



## DEVELOPMENT CONTROL COMMITTEE B

9th June 2021

### REPORT OF THE DIRECTOR: DEVELOPMENT OF PLACE

#### LIST OF CURRENT APPEALS

##### Householder appeal

Item	Ward	Address, description and appeal type	Date lodged
1	St George Troopers Hill	42 Nicholas Lane Bristol BS5 8TL A single storey extension is proposed to the rear of the property with a roof terrace accessed from the rear bedroom. Appeal against refusal Delegated decision	12/10/2020
2	Clifton	Goldney Lodge Worlds End Lane Bristol BS8 4TQ Demolition of outhouses and construction of a two storey side extension. Construction of garden studio. Appeal against refusal Delegated decision	09/04/2021
3	Redland	53 Logan Road Bristol BS7 8DS Roof extensions. Appeal against refusal Delegated decision	15/04/2021
4	Hillfields	95 Gorse Hill Bristol BS16 4PL Replacement of an existing garage with annexe for ancillary accommodation. Appeal against refusal Delegated decision	10/05/2021
5	Redland	48 Redland Court Road Bristol BS6 7EH Raise the height of roof pitch. Appeal against refusal Delegated decision	18/05/2021
6	Stoke Bishop	22 Old Sneed Avenue Bristol BS9 1SE Alterations and extensions to the property on the north (rear), west and south (road) elevations to provide additional residential accommodation. Appeal against refusal Delegated decision	18/05/2021



7	Stoke Bishop	96 Sylvan Way Bristol BS9 2LZ Proposed two storey extension Appeal against refusal Delegated decision	19/05/2021
8	Stoke Bishop	1 Ellbridge Close Bristol BS9 1BU Single storey side /rear and front extension with raised terrace. Appeal against refusal Delegated decision	19/05/2021

### Informal hearing

Item	Ward	Address, description and appeal type	Date of hearing
9	Central	Public Realm Colston Avenue Bristol BS1 4RD Temporary art installation for a period of 2 years (retrospective) entitled, 'A Surge of Power (Jen Reid) 2020' on the plinth of the former statue of slave trader Edward Colston (grade II listed). Appeal against non-determination	15/06/2021
10	Central	Public Realm Colston Avenue Bristol BS1 4RD Temporary art installation for a period of 2 years (retrospective) entitled, 'A Surge of Power (Jen Reid) 2020' on the plinth of the former statue of slave trader Edward Colston (grade II listed). Appeal against non-determination	15/06/2021
11	Lawrence Hill	1 Milsom Street Bristol BS5 0SS Enforcement notice appeal for the erection of rear extension without planning permission. Appeal against an enforcement notice	TBA

## Public inquiry

Item	Ward	Address, description and appeal type	Date of inquiry
12	Lawrence Hill	<p>Land And Buildings On The South Side Of Silverthorne Lane Bristol BS2 0QD</p> <p>Phased development of the following: site wide remediation, including demolition; (Plot 1) outline planning permission with all matters reserved aside from access for up to 23,543m<sup>2</sup> GIA of floor space to include offices (B1a), research and development (B1b), non-residential institution (D1) and up to 350m<sup>2</sup> GIA floor space for cafe (A3); (Plots 2 and 3) erection of buildings (full details) to provide 371 dwelling houses (C3), offices (B1a), restaurants and cafes (A3); (Plot 4), redevelopment of 'Erecting Sheds 1A and 1B' (full details) to provide offices (B1a); (Plot 5) erection of buildings and redevelopment of 'The Boiler Shop' (full details) to provide a 1,600 pupil secondary school (D1); (Plot 6) erection of buildings (full details) to provide 693 student bed spaces (Sui generis); infrastructure, including a new canal side walkway and associated works.</p> <p>Committee</p>	11/05/2021
13	Lawrence Hill	<p>Land And Buildings On The South Side Of Silverthorne Lane Bristol BS2 0QD</p> <p>Redevelopment of the site for: (Plot 1) Removal of the Shed 4 western gable wall; (Plot 2) Removal of Shed 4 (excluding wall to canal), insertion of opening into boundary wall and lowering/removal of material; (Plot 3) Removal of Shed 3, removal of Sheds 2a-c; (Plot 4) Insertion of pedestrian access opening into the northern boundary wall of shed 1b, alterations to the South wall of Shed 1b/north wall of Shed 2b, Restoration/rebuild of Shed 1a; (Plot 5) Reduction in height of the walls attached to the North Gateway, removal of western Hammer Forge Wall, reduction of Northern Hammer Forge Wall, demolition and rebuild of Eastern Hammer Forge wall. Works to the Boiler Shop, including new openings in the Western gable end, replacement of asbestos cement roof, removal of post-war cladding and glazing between piers, internal works including new floor level; (Plots 2-5) Potential stabilisation to the early 19th century Feeder Canal rubble stone wall.</p> <p>Committee</p>	11/05/2021
14	Bedminster	<p>Police Dog &amp; Horse Training Centre Clanage Road Bristol BS3 2JY</p> <p>Proposed change of use from training centre (Use Class D1) to touring caravan site (Use Class D2), consisting of 62 pitches and associated buildings and works.</p> <p>Committee</p>	20/07/2021

## Written representation

Item	Ward	Address, description and appeal type	Date lodged
15	Clifton Down	41 Alma Vale Road Bristol BS8 2HL Enforcement notice appeal for use of ground floor and basement levels of building as domestic storage. Appeal against an enforcement notice	14/08/2020
16	Henbury & Brentry	The Lodge Carriage Drive Bristol BS10 6TE Sycamore Tree T3 - Crown reduce canopy by a maximum of 30%. TPO 1148 Appeal against refusal Delegated decision	07/09/2020
17	Clifton	31 West Mall Bristol BS8 4BG Application for a Certificate of Existing Use/Development - use of upper floors as self contained maisonette. Appeal against refusal Delegated decision	02/11/2020
18	Eastville	15 Bridge Street Eastville Bristol BS5 6LN Enforcement notice appeal for the erection of a two storey rear extension with door access onto the roof from first floor level to rear without planning permission. Appeal against an enforcement notice	01/12/2020
19	Central	6 Pipe Lane City Centre Bristol BS1 5AJ Proposed single storey rear extension, replacement rear windows, addition of secondary glazing and internal alterations. Appeal against refusal Delegated decision	26/01/2021
20	Central	6 Pipe Lane City Centre Bristol BS1 5AJ Proposed single storey rear extension, replacement rear windows, addition of secondary glazing and internal alterations. Appeal against refusal Delegated decision	26/01/2021
21	Bishopston & Ashley Down	7 Selborne Road Bristol BS7 9PH Enforcement notice appeal for development not in accordance with the plans approved as part of planning permission 19/00729/H. Appeal against an enforcement notice	08/02/2021

22	Avonmouth & Lawrence Weston	The Glebe House 1 McLaren Road Bristol BS11 9FE Demolition of The Glebe House and the erection of five townhouses and three self-contained flats (Use Class C3) with associated car parking, cycle storage, refuse storage and private amenity space. Appeal against refusal Delegated decision	25/02/2021
23	Southville	6 Albany Buildings Bristol BS3 1BT Works and a change of use to facilitate the conversion of an existing building to two residential units. Appeal against non-determination Delegated decision	25/02/2021
24	Knowle	100 Newquay Road Bristol BS4 1DS Change of use from vacant off-licence (A1) to a Hot Food Takeaway (Sui Generis). Appeal against refusal Delegated decision	01/03/2021
25	Southmead	Greystoke Avenue Repton Grange Bristol BS10 5NZ Application to determine if prior approval is required for a proposed telecommunications installation: 15m high Phase 8 monopole, C/W wraparound Cabinet at base and associated ancillary works. Appeal against refusal Delegated decision	12/03/2021
26	Brislington West	57 West Town Lane Brislington Bristol BS4 5DD New single storey dwelling. (Self Build). Appeal against refusal Delegated decision	15/03/2021
27	Clifton Down	41 Alma Vale Road Clifton Bristol BS8 2HL Change of use from D1 (pre-school care) to C3 flat. (Ground floor and part basement). Appeal against refusal Delegated decision	25/03/2021
28	Eastville	4 Island Gardens Bristol BS16 1BU Application for Outline Planning Permission With Some Matters Reserved for two dwelling houses. Approval sought for Access, landscaping, Layout, Scale. Appeal against refusal Delegated decision	30/03/2021

29	Bishopsworth	Telecommunication Cabinet Lake Shore Drive Bristol BS13 7BA Proposed telecommunications installation: Proposed 20m Phase 8 Monopole C/W wrapround Cabinet at base and associated ancillary works. Appeal against refusal Delegated decision	30/03/2021
30	Stoke Bishop	13 Avon Grove Bristol BS9 1PJ Proposed detached garage. Appeal against non-determination Delegated decision	12/04/2021
31	Stockwood	Land Front Of Stockwood Precinct Stockwood Road Stockwood Bristol Application to determine whether prior approval is required for the proposed installation of a 20 metre high telecommunications column supporting 6 no. antennas, together with ground-based equipment cabinets and ancillary development. Appeal against refusal Delegated decision	13/04/2021
32	Lockleaze	39 Stothard Road Bristol BS7 9XL x2 two storey 2 bedroom, 3 person semi-detached dwellings to front of site and x2 single storey 2 bedroom, 3 person dwellings to rear of site as well as access and x6 car parking spaces, bin and cycle storage. Appeal against refusal Delegated decision	10/05/2021
33	Clifton Down	All Saints Court All Saints Road Bristol BS8 2JE Refurbishment of existing building. Conversion of garages to form 2no. (1bedroom 2 bedspace) Use Class C3 residential units. Extension to form 2 (1bedroom 2 bedspace) Use Class C3 residential units at roof level. Erection of bin and cycle stores. Appeal against non-determination	10/05/2021
34	Avonmouth & Lawrence Weston	130 Nibley Road Bristol BS11 9XN Enforcement notice appeal for use of building to rear for operation of motorbike repair business. Appeal against an enforcement notice	17/05/2021
35	Bedminster	1 - 21 Bedminster Down Road Bristol BS13 7AB Removal of 4no. hoarding panels and installation of 1no. single sided, illuminated, sequential, landscape display. Appeal against refusal Delegated decision	18/05/2021

36	Filwood	10 Melvin Square Bristol BS4 1LZ Part demolition of existing building and erection of 6no. apartments (use class C3) with secure cycle parking and refuse/recycling store. Appeal against refusal Delegated decision	21/05/2021
37	St George Central	75 New Queen Street St George Bristol BS15 1DE Demolition of existing garage and outbuildings. Two storey side extension to form separate dwellinghouse (Use Class C3) including new vehicle access and driveway with 4no. off-street parking spaces. Appeal against refusal Delegated decision	25/05/2021
38	Avonmouth & Lawrence Weston	Garage Hung Road Site Adjacent To 125 Nibley Road Bristol Creation of a 1 x 2 bedroom property within the side plot of no.125 Nibley Road. Appeal against refusal Delegated decision	25/05/2021
39	Clifton Down	Beer Garden For Brewhouse And Kitchen Cotham Hill Bristol BS6 6JY Retrospective Application for the installation of new retractable awning to existing beer garden opposite Brewhouse & Kitchen. Appeal against refusal Delegated decision	25/05/2021
40	Avonmouth & Lawrence Weston	Beaumont Court Avonmouth Road Bristol BS11 9FL Creation of additional apartments at third-floor. Appeal against refusal Delegated decision	26/05/2021

### List of appeal decisions

Item	Ward	Address, description and appeal type	Decision and date decided
41	Frome Vale	67 Symington Road Bristol BS16 2LN One bedroom single storey dwelling in the rear garden of the existing property. Appeal against refusal Delegated decision	Appeal dismissed 22/04/2021

42	Southville	Former Pring And St Hill Ltd Malago Road Bristol BS3 4JH Redevelopment of the site to provide 74 No. student cluster units and 40 No. affordable housing units (social rented), flexible ground floor community/commercial use (Use class A1-A5/D1/B1). Landscaping , access and public realm works and associated works to the Malago Road. (Major Application) Appeal against refusal Committee	Appeal dismissed 01/04/2021
43	Southville	Former Pring And St Hill Ltd Malago Road Bristol BS3 4JH Redevelopment to provide student accommodation across four development blocks, landscaping, access, public realm works and associated works to the Malago River. Appeal against non-determination Delegated decision	Appeal dismissed 01/04/2021
44	Avonmouth & Lawrence Weston	6 Springfield Lawns Station Road Shirehampton Bristol BS11 9TY 6 x Lawson Cypress - Felling including stubbing out to the rear of 6 Springfield Lawns. TPO 097. Appeal against refusal Delegated decision	Appeal dismissed 21/04/2021
45	Eastville	12 Lodge Causeway Bristol BS16 3HY Change of use from existing family dwellinghouse (C3) to a House of Multiple Occupation (HMO) with 8 bed-spaces (sui generis), incorporating a single-storey rear extension and all associated works. Appeal against refusal Delegated decision	Appeal allowed 19/05/2021  Costs not awarded
46	Southville	215 North Street Bedminster Bristol BS3 1JH Demolishing existing single storey building and replacement with new two storey residential unit. Appeal against refusal Delegated decision	Appeal allowed 27/04/2021
47	Westbury-on-Trym & Henleaze	7A North View Bristol BS6 7PT Erection of rear roof extension to create 1 no. new flat (Class C3) - resubmission of planning application ref: 19/05608/F. Appeal against refusal Delegated decision	Appeal dismissed 18/05/2021
48	Hengrove & Whitchurch Park	32 Widcombe Bristol BS14 0AS Erection of new 2 bed dwelling attached to side of existing house. Appeal against refusal Delegated decision	Appeal dismissed 24/05/2021

49	Westbury-on-Trym & Henleaze	Orange Mast Lime Trees Road Bristol BS6 7XW Application to determine if prior approval is required for a proposed - Monopole and cabinets to be installed. Appeal against refusal Delegated decision	Appeal allowed 24/05/2021
50	Hillfields	6 Esson Road Bristol BS15 1NP Proposed two storey rear extension. Appeal against refusal Delegated decision	Appeal dismissed 13/04/2021
51	Eastville	4 Island Gardens Bristol BS16 1BU Application for a Certificate of Proposed Development - detached outbuilding. Appeal against refusal Delegated decision	Appeal allowed 09/04/2021
52	St George Central	278 Two Mile Hill Road Bristol BS15 1AT Formation of dropped kerb. Appeal against refusal Delegated decision	Appeal allowed 13/04/2021
53	Stockwood	211 Whittock Road Bristol BS14 8DB Proposed 2 bed, two storey single dwelling house, attached to the side of the existing property. Appeal against refusal Delegated decision	Appeal dismissed 09/04/2021
54	Cotham	Ground Floor Flat 3 Victoria Walk Bristol BS6 5SR New window to side elevation. Appeal against refusal Delegated decision	Appeal dismissed 12/04/2021
55	Clifton Down	Whiteladies Residential Home 22 Redland Park Bristol BS6 6SD Proposed demolition of coach house and conversion of Nursing Home into one 1 bedroom (2 bedspace) flat four 2 bedroom (3 bed space) flats, two 2 bedroom (4 bed space) flats, one 3 bedroom (6 bed space) flat and the rebuilding of the two storey coach house to form a new 2 bedroom (4 bed space) cottage, with associated bin and cycle storage and parking. Appeal against refusal Delegated decision	Appeal allowed 29/04/2021



56	Ashley	<p>The Jamaica Inn 2 - 4 Grosvenor Road Bristol BS2 8XW</p> <p>Outline planning application (including consideration of Access, Appearance, Layout and Scale) for the demolition of the existing building and the erection of 10 no. self-contained flats (Use class C3) with associated cycle storage, private amenity space and refuse storage.</p> <p>Appeal against refusal</p> <p>Delegated decision</p>	<p>Appeal dismissed</p> <p>27/04/2021</p>
57	Redland	<p>125 Redland Road Bristol BS6 6XX</p> <p>Replace existing upvc top hung dormer windows at second floor level and replace with enlarged dormer windows with side hung timber windows.</p> <p>Appeal against refusal</p> <p>Delegated decision</p>	<p>Appeal dismissed</p> <p>29/04/2021</p>
58	Henbury & Brentry	<p>The Hazels Sheepwood Road Bristol BS10 7BS</p> <p>T3 conifer cypress - Fell.(TPO 398)</p> <p>Appeal against refusal</p> <p>Delegated decision</p>	<p>Appeal dismissed</p> <p>17/05/2021</p>
59	Horfield	<p>TA Centre Dorian Road Bristol BS7 0XL</p> <p>The installation of supporting steelwork accommodating 6no antenna apertures and 2no transmission dishes, plus 4no equipment cabinets and ancillary development thereto. As part of this application, 2no existing telecommunications flagpoles (measuring 5m and 7m, respectively), and two existing equipment cabinets, will be removed.</p> <p>Appeal against refusal</p> <p>Delegated decision</p>	<p>Appeal allowed</p> <p>24/05/2021</p>
60	Hartcliffe & Withywood	<p>60 Gatcombe Road Bristol BS13 9RD</p> <p>New attached dwelling to existing terrace.</p> <p>Appeal against refusal</p> <p>Delegated decision</p>	<p>Appeal dismissed</p> <p>22/04/2021</p>
61	Westbury-on-Trym & Henleaze	<p>32 Parrys Lane Bristol BS9 1AB</p> <p>Partial change of use from residential C3 to D1 for dentistry purposes on the ground floor. Proposed part single storey/part double storey extension and alterations the existing house to maintain C3 dwelling unit above. Creation of new access from Elmlea Avenue and alteration of existing access from Parrys Lane. Creation of car park to front and side of property.</p> <p>Appeal against refusal</p> <p>Delegated decision</p>	<p>Appeal allowed</p> <p>19/05/2021</p>

62	Westbury-on-Trym & Henleaze	28 South Croft Bristol BS9 4PR Proposed hip to gable roof alteration and rear dormer roof extension. Appeal against refusal Delegated decision	Appeal allowed 21/05/2021
----	--------------------------------	---	------------------------------

## DEVELOPMENT CONTROL COMMITTEE B

### 9th June 2021

#### REPORT OF THE DIRECTOR: DEVELOPMENT OF PLACE

##### LIST OF ENFORCEMENT NOTICES SERVED

Item	Ward	Address, description and enforcement type	Date issued
1	Ashley	58 Banner Road Bristol BS6 5LZ  Use of basement (formation) of separate flat without planning permission. Enforcement notice	11/05/2021
2	Hillfields	113 Thicket Avenue Bristol BS16 4EQ  The erection of two canopy structures to the front of the property. Enforcement notice	18/05/2021
3	Hotwells & Harbourside	63 Park Street City Centre Bristol BS1 5NU  Installation of shipping container. Enforcement notice	18/05/2021
4	St George Troopers Hill	Trooper Court 303 Crews Hole Road Bristol BS5 8BQ  Installation of timber enclosure without planning permission. Enforcement notice	05/05/2021

## Development Control Committee B 9 June 2021

### Report of the Director: Development of Place

#### Index

#### Planning Applications

Item	Ward	Officer Recommendation	Application No/Address/Description
1	Redland	Refuse	20/06030/F - 7 Belvedere Road Bristol BS6 7JG Proposed change of use from 3 No. residential flats to provide 14 No. additional Bed spaces to Glenview Nursing Home at 8-9 Belvedere Road, Bristol.
2	Windmill Hill	Grant subject to Legal Agreement	20/04678/F - St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS Redevelopment of existing site with demolition of existing NHS healthcare facility and William Hill building (no. 217) to create an affordable housing complex, (36 units).

index  
v5.0514

## Development Control Committee B – 9 June 2021

ITEM NO. 1

**WARD:** Redland

**SITE ADDRESS:** 7 Belvedere Road Bristol BS6 7JG

**APPLICATION NO:** 20/06030/F Full Planning

**DETERMINATION DEADLINE:** 11 June 2021

*Proposed change of use from 3 No. residential flats to provide 14 No. additional Bed spaces to Glenview Nursing Home at 8-9 Belvedere Road, Bristol.*

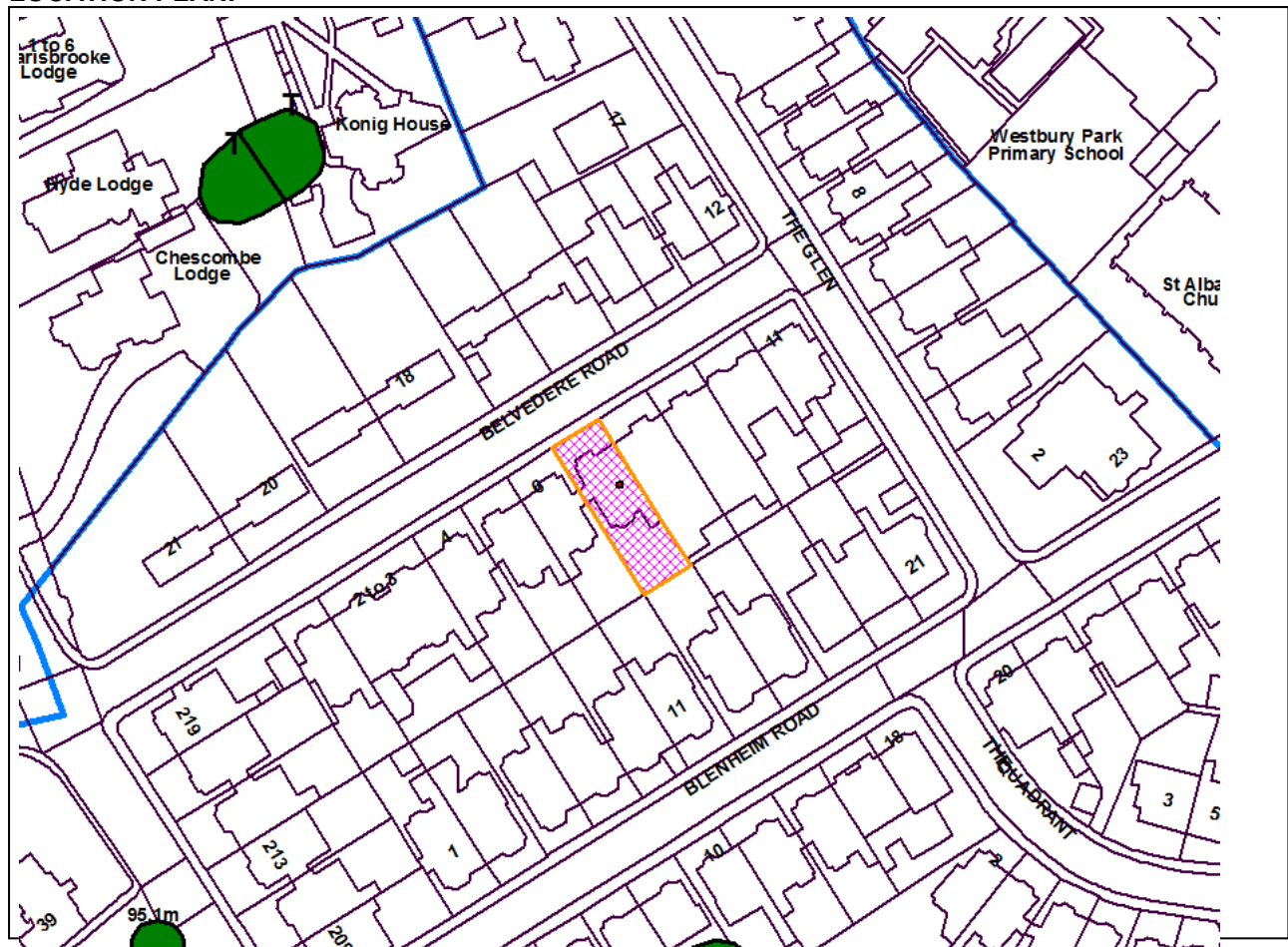
**RECOMMENDATION:** Refuse

**AGENT:** Hadfield + Noblin Architects Ltd  
The Old Brushworks  
56 Pickwick Road  
Corsham  
SN13 9BX

**APPLICANT:** Meadowcare Homes  
2-3 Belvedere Road  
Corsham  
BS6 7JG  
United Kingdom

*The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.*

### LOCATION PLAN:



**Development Control Committee B – 9 June 2021**  
**Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

## SITE DESCRIPTION AND BACKGROUND

This application relates to a three-storey building known as 7 Belvedere Road, in Redland, north Bristol.

The building is split into three flats: a maisonette at ground floor and basement level; a flat at first floor level and another flat at second floor level. Each unit is in residential use under Use Class C3 and has two bedrooms. Access to the building is maintained through a single storey porch to the side of the building and via the front of the building.

The building is constructed in pennant stone and render and features a large bay window at the front elevation.

Belvedere Road includes a number of identical, large, late-Victorian townhouses which have largely been subdivided into maisonettes or flats, with three no. elderly peoples' homes / assisted living units. Historic maps indicate that 7 Belvedere Road was constructed around 1900.

Belvedere Road is a two-way street, lined on both sides with unallocated car parking. The site is on the outside edge of the Cotham North Residents' Parking Scheme.

The site is located within the Downs Conservation Area. The surrounding area is almost entirely residential in character but the site is in close proximity to Westbury Park School and a number of care homes on Westbury Park, Belvedere Road, the Glen and Redland Road.

In June 2020, full planning permission was refused by Development Control Committee B for the conversion of the three flats at 7 Belvedere Road to a 17-bed extension to the nursing home at 8-9 Belvedere Road (Use Class C2, residential institution).

The application for change of use was refused by Development Control Committee B for the following reasons:

1. The proposed development would result in an unacceptable increase in demand for parking, leading to inappropriate on-street parking activities, safety concerns and the obstruction of access to private driveways. This would be contrary to Policy BCS10 (Transport and Access Improvements), Policy DM2 (Residential Sub-divisions, Shared and Specialist Housing) and Policy DM23 (Transport Development Management).
2. The proposed development would result in an overconcentration of residential institutions on Belvedere Road, which would lead to harm to the mix, balance and inclusivity of the community, contrary to Policy BCS18 (Housing Type) and reduce the choice of homes in the area by changing the housing mix contrary to Policy DM2 (Residential Sub-divisions, Shared and Specialist Housing).
3. The proposed development would result in a harmful concentration of shared housing / care homes on Belvedere Road, worsening the existing harmful conditions listed within point (i) of Policy DM2 (Residential Sub-divisions, Shared and Specialist Housing), including excessive noise and disturbance and inadequate storage of recycling/refuse.

An appeal (ref. APP/Z0116/W/20/3263935) was submitted to the Planning Inspectorate (PINS) by the applicant in response to the decision to refuse the above application. This appeal was dismissed by PINS on the basis that the scheme would result in an unacceptable impact on highway safety.

This current application was to be considered by the Development Control Committee B on the 17 March 2020, however the appeal decision was received between the scheme being presented at DC

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

Committee Agenda Briefing and the date of the Committee Meeting and officers withdrew the application from the Agenda to allow for consideration of the dismissed appeal.

**APPLICATION**

This application seeks full planning permission for the conversion of the existing building known as 7 Belvedere Road, containing three residential units (Use Class C3) to create a residential institution (use class C2) for people who require nursing or personal care.

The proposed development would result in the rearrangement and subdivision of the internal spaces within the building to form 14 no. bedrooms. The building would form part of the adjacent nursing home, with new openings proposed to be created in the side-facing wall between 7 and 8 Belvedere Road at lower ground, upper ground, first and second floors.

At lower ground level, the proposed converted building would consist of a laundry, a lounge and two no. bedrooms. Each of the bedrooms would be afforded with an en-suite bathroom.

At upper ground floor level, the proposed care home would consist of four no. bedrooms, each with an en-suite bathroom. A lounge area would be provided towards the front of the building. Access to this part of the building would be retained through the porch to the side of the building. The entrance to the front of 7 Belvedere Road would be replaced with a window.

A further 8 bedrooms are proposed to be split across the first and second floors. Each of these bedrooms would have an en-suite bathroom and at least one window. Each floor would also include a lounge area for residents.

External alterations to the building are limited to the replacement of the front entrance with a window, the increase in size of two side-facing windows and a dormer to the side roof. The proposed development would also require some excavation to create lightwells down to basement bedrooms / living areas.

The garden to the rear of the building would be retained, with access to this area created through the lounge at basement level.

Cycle parking for residents and staff would be proposed to the front of the building in the form of a three space 'Asgard' secure container. A smoking area for staff is proposed to the front of 7 Belvedere Road.

An additional bin store (further to the existing one provided as part of the adjacent nursing home at 8-9 Belvedere Road) would be situated to the front of the building.

Landscaping is proposed along the frontage with Belvedere Road to reduce the visual impact of the proposed smoking area and bin and cycle stores.

**RELEVANT HISTORY****Application site:**

APP/Z0116/W/20/3263935 - Appeal against a refusal to grant planning permission for application ref. 19/03104/F. – APPEAL DISMISSED. (March 2021)

19/03104/F - Change of use from 3 x flats to a 17 x bed extension to the nursing home at 8-9 Belvedere Road. – REFUSED. (June 2020)

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

This application was refused by DC Committee as per the reasons set out in the Background section of this report.

18/03500/F - Extension of existing basement level to create enlarged single residential dwelling (use class C3) – GRANTED subject to condition(s). (September 2018.)

17/04752/F - Change of use from 3 x flats to a 17 x bed extension to the nursing home at 8-9 Belvedere Road. External alterations to building including rear extension and side and rear dormer roof extension. – WITHDRAWN. (November 2017)

This application was withdrawn due to concerns about the impact of the proposals on the highway network, the impact upon the appearance of the building and the surrounding Conservation Area and the absence of a justification for the loss of the three flats.

**Adjacent sites:**

08/02673/F - Construction of a new central entrance area linking Nos 8 & 9 Belvedere Road and extension to first and second floors above, and single storey rear extension to form a 40 bed nursing home. – GRANTED subject to condition(s).

**RESPONSE TO PUBLICITY AND CONSULTATION****Public comments**

A total of 221 public comments were received in response to this application.

A total of 109 comments were received in support for the proposed development. Approximately 103 of these comments were submitted via the Applicant (i.e. via the Applicant's email address). These comments set out the following reasons in support:

- The proposals would provide much-needed healthcare provision.
- The residents of the care home would have less demand for car parking spaces than existing residents.

A total of 112 comments were received in objection. The following planning issues were raised:

- Concerns about the impact of the proposed development on local parking provision and associated highways safety.
- Concerns about the impact of the proposed development on local congestion, including by emergency vehicles and delivery vehicles.
- Concerns about waste disposal and its impact on street scene and local amenity.
- Concerns about potential noise, disruption and dust that would be caused by construction.
- Concern about the effect of the proposed development on the Conservation Area and heritage.
- Concern that the proposal would result in a disproportionate intensification of the site's existing use.
- Concern about loss of outlook to the detriment of residential amenity.
- Concern about the impact on the balance and character of this residential neighbourhood.
- Concern that the proposed development would involve a loss of residential housing for the local community
- The proposals are not sufficiently different from those refused at DC Committee in 2020.

The following non-planning issues were raised:

- The quality of care being provided.
- The effect of building work on land stability.

**Councillor Referral to Committee**

Councillor Fodor referred the application to Committee for the following reasons:



**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

This address and a similar application was rejected at committee in 2020 but a revised application has been resubmitted with fewer bed spaces. The planning authority is currently preparing to defend the committee decision from 2020.

However a new application has been submitted. I am therefore calling this in line with the wishes of many residents and wish to ensure the cumulative impacts of the change of use of this residential street from 3 units of housing into an extended series of Care Home businesses be considered at committee.

While this is slightly smaller an application than the one rejected last year the proposals would still lead to a permanent increase in traffic and parking problems in a street unable to cope (and already subject to significant overspill parking from RPS areas nearby). The traffic implications of the deliveries, staff, emergency vehicles and visitors to the various care homes all need to be considered. There will be additional noise and disturbance from the operation of this business in a residential street, such as recent incidents of residents' blocked drives.

The impacts on heritage in the conservation area are also grounds for concern.

In addition the construction traffic impacts, noise and dust, will be a problem.

I therefore ask for it to be considered at committee if it is to be recommended for approval so that all sides of the story can be heard by members. Please notify me of the proposed committee date if it does get recommended for approval and then has to be timetabled to be heard there.

**Internal consultees**

Transport Development Management – No objection

**Highway Network**

The site is located to the edge of the Cotham North residents parking scheme in which on street parking in this location is at a premium. There are a number of bus services which are available within walking distance of the site; the number 1, 2, 2a, 3 and U1 services operate from the Durdham Park bus stops with the 505 service operating along Coldharbour Road. The 3x and 72a services operate from the Chapel Green Lane bus stops which are approximately a 560m walk from the site.

The site is located approximately 700m from Whiteladies Road which is considered to be a popular busy cycle route offering options for cycling towards the City Centre. The Downs Way (NCN4) offers a traffic free/Quiet Road route in close proximity to the site offering routes both South and North. A number of other cycle routes are situated within the sites vicinity promoting cycling as a feasible alternative to the private car.

**Car Parking**

The applicant has undertaken parking surveys across three days. These were undertaken following consultation with TDM with the following times on each day surveyed:

- 07:00
- 08:00
- 13:30
- 14:30
- 19:30
- 20:30

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

It should be noted these times coincide with the staff changeover times given this represents the worst case scenario when assessing the impact of the existing care home and the proposed care home.

The parking survey results demonstrate the 07:00 and 08:00 to be the peak for parking demand. The parking survey also demonstrates significant parking stress along Blenheim Road and The Quadrant throughout the day with an oversubscription of parking across all surveys. There is however a number of free parking spaces within 200m of the site, predominantly along Westbury Park.

A parking survey has also been undertaken by the local residents. This also demonstrates the parking constraint experienced within the local vicinity of the site. The results of these surveys demonstrate near full parking earlier within the day with particular constraint around school pick up times. There tends to be more spaces available during the evening.

The appeal for the previous application was dismissed on the basis the development would harm highway safety following consideration of the reduction in car parking survey and car ownership as a result of the proposal, the snapshot nature of the parking surveys and the requirement for loading bays. As referenced within the appeal decision 'the parking situation results in vehicles often having to park in the middle road. This causes congestion and conflict which is exacerbated by two way working and creates a hazard for all road users'.

TDM raise concerns regarding an intensification of parking in this area. Whilst there may be a slight reduction in demand for visitor parking associated with the reduction in beds from 17 to 14, this is not considered sufficiently different to the refused scheme and it is concluded that this proposal would worsen an existing situation where on street parking is at a premium. It would therefore be considered to harm highway safety.

#### Servicing

The applicant is proposing to install two loading bays along Belvedere Road to address the current issues surrounding servicing of the development. The appeal decision noted that the installation of these bays would help to avoid emergency services / deliveries stopping in the middle of the road, however they would also be in high demand for the variety of proposed servicing uses and would result in a further loss of on-street parking, which would worsen the existing situation.

TDM concurs with this conclusion, and considers that the proposed solution to install two loading bays is unlikely to be workable and likely to reduce the availability of parking for other road users.

The provision of a further area for waste storage is welcomed and it is recommended that a condition is attached to any permission to ensure that bins are stored within this area and only left out on collection days.

#### Cycle Parking

Three cycle parking spaces are to be provided within an Asgard style cycle locker as demonstrated within plan 033:003. This is considered to be an appropriate provision.

#### Travel Planning

The travel plan has not been revised from the 19/03104/F submission. The travel plan co-ordinator has reviewed the submitted travel plan and raises no concerns given the development falls below the threshold as per the travel plan guidance for new developments.

#### Construction Management/ Network Management

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

Due to the impact this proposal would have on the highway network during the demolition/construction period, the applicant would be required to produce and submit a highway network construction management plan. There is a construction method statement submitted in support of the previous scheme, not the scheme submitted under application number 20/06030/F. TDM therefore required condition B3 be applied, should the application be approved.

**Recommendation**

It is recommended the application is refused on the basis that the proposed development would generate further demand for on-street parking in an area which is already oversubscribed and would subsequently result in harm to highway safety.

**City Design Group – No objection (Surgery Item)**

The proposed external alterations are considered to be minor in nature, and the use of landscaping to reduce the visual impact of bin and bike stores is supported. It is recommended that a landscaping / planting plan is secured via condition.

The proposed excavation to increase the area of the basement of level is considered to have a limited impact upon the Conservation Area given the extent to which this has been implemented elsewhere on Belvedere Road at no's. 8 to 11 adjacent.

**RELEVANT POLICIES**

National Planning Policy Framework – February 2019

Bristol Local Plan, Comprising:

- Core Strategy (Adopted June 2011),
- Site Allocation and Development Management Policies (Adopted July 2014).

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

**KEY ISSUES****A. IS THE PROPOSED DEVELOPMENT ACCEPTABLE IN PRINCIPLE?**

Section 5 of the National Planning Policy Framework (NPPF) outlines that housing applications should be considered in the context of the presumption in favour of sustainable development.

Policy BCS5 sets out that the Core Strategy aims to deliver new homes within Bristol's existing built up areas to contribute towards accommodating a growing number of people and households in the city. In order to maintain the net housing stock, the Policy states that existing homes will be retained unless they are unsuitable for residential uses, would be used for essential local community facilities or would be replaced.

Policy BCS12 states that community facilities should be located where there is a choice of travel options and should be accessible to all members of the community. Where possible community facilities should be located within existing centres.

Policy BCS20 states that development should maximise opportunities to re-use previously developed land.

On 19 January 2021, the government published the results of its 2020 Housing Delivery Test, which aims to measure how effectively each local authority is delivering housing against NPPF requirement to demonstrate a five-year supply of deliverable housing sites plus five per cent land supply buffer.

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

Bristol was found to be delivering only 72% of the housing requirement. The penalties for this will be that Bristol will be required to provide a "buffer" of sites for 20% more homes than are needed to meet its five-year target, to produce a Housing Action Plan, and that the presumption in favour of development in the NPPF will apply.

In view of the fact that the LPA is not able to demonstrate a five year housing land supply, paragraph 11(d) of the NPPF is engaged, and the tilted balance applies to applications which result in the gain or loss of housing units.

The proposed change of use would result in the loss of three residential flats. Whilst the loss of the three flats would be to the detriment of Bristol's housing stock, the proposed development would retain the overarching residential nature of the property by providing living accommodation for 14 occupants. The proposed change of use would help to address the increasing demand for dementia care at both a national and local level, as set out within the Applicant's Needs Assessment Report (London Care & Support Forum, March 2019).

It is considered that when compared to the overall housing stock within the Redland area, the loss of three residential units is not significant and that the need for dementia care outweighs this loss. The tilted balance has been considered in this context and the loss of the housing units is sufficiently justified in this case.

In conclusion, the proposed development is considered to be acceptable in principle.

#### **B. WOULD THE PROPOSED DEVELOPMENT RESULT IN HARM TO THE MIX AND BALANCE OF THE COMMUNITY?**

Policy BCS18 states that all new residential development should maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities.

Policy DM2 of the Site Allocations and Development Management Policies states that proposals for shared housing, such as older persons' housing, should not result in a harmful concentration of such uses within the locality as a result of reducing the choice of homes in the area by changing the housing mix.

It is understood that the existing nursing home at 8-9 Belvedere Road has a maximum occupancy of 40 residents. The proposed change of use of 7 Belvedere Road would increase this to 54 persons in total. This represents a 35% increase in the size of the nursing home.

The proposed development would result in the loss of three residential flats, each with two bedrooms.

At the DC Committee in April 2020, members raised concerns that the development proposed as part of application 19/03104/F could result in harm to the mix and balance of the community, as per the requirements of Policy BCS18. Whilst this development is not a residential proposal (Use Class C3), the proposed development does provide living accommodation and therefore the residential mix of the Lower Super Output Area (LSOA) and Belvedere Road has been assessed.

The Manor Park LSOA consists of 2,049 residents at the time of the 2011 Census, of which there were 109 residents living within 'communal establishments', equivalent to 5.3% of the population. If a further 14 care home residents were to be added to the population, this would increase to 5.9%.

It has been noted within a number of submissions by local residents that the density of care home residents is significantly higher when measured on the basis of just Belvedere Road. The submissions have indicated a range of estimates from in excess of 20% up to nearly 50% of residents on Belvedere Road residing as care home residents.

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

It should be noted by Members that Policy DM2 does not define what percentage an overconcentration would constitute. Officers cannot speculate whether the estimates provided by local residents are correct and therefore consider that the harmful concentration should be assessed not on the basis of the mix of the area / street, but by assessment of the harmful conditions set out within Policy DM2. The assessment against these conditions is set out within the remainder of this committee report.

It is considered that the proposed development would not sufficiently reduce the choice of homes in the area to warrant refusal for the following reasons. Firstly, the proposed development would result in the loss of just three dwellings. This is less than 0.03% of the total housing stock within the Manor Park LSOA. Secondly, two bedroom dwellings (of which three are proposed to be lost), make up over 39% of the housing stock. This is significantly above the Bristol average of 28%. No 'family housing', i.e. dwellings of three bedrooms or over, is to be lost.

It is considered that the proposed development would not cause harm to the mix and balance of the community by reducing the housing stock.

The remaining assessment against Policy DM2, which states that development that would create or contribute to a harmful concentration of such uses [shared housing] within a locality as a result of: exacerbating harmful conditions is considered in Key Issue D (noise and disturbance), Key Issue B (levels of on-street parking that cannot be accommodated and inadequate storage of recycling and refuse), and Key Issue C (harmful physical alterations).

#### **C. IS THE IMPACT OF THE PROPOSED DEVELOPMENT UPON TRANSPORT AND HIGHWAYS ACCEPTABLE?**

Policy BCS10 states that developments should be designed and located to ensure the provision of safe streets. Development should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

Policy DM2 states that proposals for shared forms of housing will not be permitted where levels of on-street parking cannot be accommodated or regulated, or if there is insufficient storage for recycling/refuse and cycles.

Policy DM23 outlines that development should not give rise to unacceptable traffic conditions and will be expected to provide safe and adequate access onto the highway network.

The proposed development would generate two additional staff, with this reducing to one additional member of staff overnight. The Travel Plan submitted by the applicant indicates that the proposed extension would result in up to 1 additional car parking space being required to accommodate staff. Three lockable cycle spaces would be provided for staff.

It is estimated that two additional visitors' cars would be parked on street per day. It is understood that visiting times are typically late morning and early afternoon.

The Travel Plan indicates that there would be two no. additional supply deliveries as a result of the proposed development. The changes to refuse collections would be limited given the existing collections from 7 and 8 Belvedere Road.

For this Application, the Applicant has agreed a parking survey methodology with Transport Development Management and undertaken the survey in accordance with BCC's Parking Survey Methodology. Parking Surveys were taken across three days, with snapshots taken at 7am, 8am, 1.30pm, 2.30pm, 7.30pm and 8.30pm. These times were agreed as they coincide with the staff changeover times given these represent the worst case scenario when assessing the impact of the existing care home and the proposed care home.

**Development Control Committee B – 9 June 2021**  
**Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

A Parking Survey has also been provided by the Westbury Park Community Association (WPCA). These timings differ slightly, having been undertaken as a late morning (10.45am, 11am and 12pm), mid-afternoon (3pm, 3.20pm and 3.40pm) and an evening (7pm, 7.30pm and 7.45pm) snapshot.

Both parking surveys identify significant demand for parking in the area. This includes in particular, Blenheim Road and the Quadrant, which are often oversubscribed. Both parking surveys corroborate that the greatest parking demand occurs in the morning.

The appeal for the previous application was dismissed on the basis the development would harm highway safety following consideration of the reduction in car parking survey and car ownership as a result of the proposal, the snapshot nature of the parking surveys and the requirement for loading bays. As referenced within the appeal decision “the parking situation results in vehicles often having to park in the middle road. This causes congestion and conflict which is exacerbated by two way working and creates a hazard for all road users”.

Concerns are raised regarding any increase in car parking demand in this area given the severe constraints on on-street car parking. The reduction from 17 to 14 beds is not considered to be sufficient to address concerns of PINS from the previous scheme. It is likely that the lack of available car parking spaces would result in inconsiderate parking and a lack of space for passing given the two-way movement. This would therefore be considered to harm highway safety.

Turning to the issue of deliveries and servicing, it is noted that a significant number of submissions by local residents provide evidence of delivery vehicles and ambulances blocking Belvedere Road and inconsiderate parking by those vehicles. In order to respond to the additional demand created by the proposed development, the applicant has agreed to install two loading bays along Belvedere Road.

There is a concern however that the loss of four on-street parking spaces as a result of the proposed loading bays would increase parking stresses on Belvedere Road. This would again encourage inconsiderate or unsafe parking and increase conflict between two-way movements.

Should the application be approved, the Applicant would need to provide a Delivery & Servicing Management Plan, secured by condition. This would require the applicant to provide details of a qualified co-ordinator, for controls to be placed on movements, parking and waiting, and for an approach to be taken to minimise disruption.

The proposed development would include an additional area for refuse and recycling. Submissions made by local residents highlight that there are issues with the existing refuse arrangements at 8-9 Belvedere Road. This includes overflowing bins and placement of waste in the incorrect receptacles. Officers consider that the additional waste provision would help to address some of issues of overflowing bins and it is recommended that a condition is attached to any permission to ensure that bins are stored within this area and only left out on collection days.

A Waste Management Plan is also to be secured via condition, should the application be approved. This would require a proper plan for collection and management of waste to be in place and should help to address some of the issues highlighted by residents at the existing care home.

The proposals for cycle storage are considered to be acceptable.

The travel plan has not been revised from the 19/03104/F submission. The travel plan co-ordinator has reviewed the submitted travel plan and raises no concerns given the development falls below the threshold as per the travel plan guidance for new developments.

A construction management plan for the proposed development and a construction method statement for the basement extension should be conditioned.

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

In conclusion, it is considered that the proposed development would worsen existing stresses on the public highway as a result of parking demand in the area. This would lead congestion and conflict between road users and would result in harm to highway safety, and as such, the proposed would be contrary to Policies BCS10 (Transport and Access Improvements) and DM23 (Transport Development Management).

**D. WOULD THE PROPOSED DEVELOPMENT BE OUT OF SCALE OR CONTEXT WITH THE CONSERVATION AREA?**

Paragraph 193 of the National Planning Policy Framework (NPPF) outlines that:

“When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset’s conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.”

Paragraph 196 of the NPPF states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.

Policy BCS20 sets out that an appropriate density should be informed by the characteristics of the site and the local context.

Policy BCS21 advocates that new development should deliver high quality urban design that contributes positively to an area's character and identity, whilst safeguarding the amenity of existing development.

Policy BCS22 and Policy DM31 state that development proposals will safeguard or enhance heritage assets such as the character and setting of Conservation Areas.

The proposed development would result in the loss of the existing front door and replacement with a sash window to match the adjacent windows, the construction of two dormers (one to the side elevation and one to the rear elevation) and excavation of part of the front and rear curtilage of the site to create lightwells.

On the basis of the adjacent buildings on Belvedere Road which are of uniform type and appearance, it would appear that the front door is a later addition to 7 Belvedere Road, with access originally gained via the single storey porch to the side of the building. As such, it is considered that the proposed removal of this door and replacement with a sash window to match the adjacent windows would enhance the character of the dwelling and in turn provide a benefit to the character of the Conservation Area by returning it back to its historic appearance.

The proposed side dormer would sit comfortably within the roof slope given its small nature and would have a limited impact upon character of the building or the Conservation Area.

The proposed landscaping to the front of the building would minimise the impact of the proposed bin stores and cycle store.

The proposed excavations would have a less than substantial harm on the character of the building and the Conservation Area given the existing prevalence of such lightwells and basement extensions. This would be outweighed by the public benefit of providing care home accommodation.

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

Conditions should be attached to any recommendation for approval to secure construction details of the proposed new window, details of materials for the proposed window and the new dormers and for the submission of a detailed landscaping scheme.

It is considered that the proposed development would be in keeping with the character of the Conservation Area.

**E. WOULD THE PROPOSED DEVELOPMENT CAUSE ANY UNACCEPTABLE HARM TO RESIDENTIAL AMENITY?**

Policy BCS21 states that new development should safeguard the amenity of existing development.

Policy DM2 states that where possible, older persons' housing should meet the Lifetime Homes Standards and 20% of the units should be wheelchair accessible or adaptable for wheelchair users.

Policy DM30 states that proposals should not prejudice the existing and future development potential of adjoining sites.

The proposed development consists of only minor external alterations to the existing building which are unlikely to have an impact upon neighbours in terms of overlooking, overshadowing or creating a sense of overbearing.

The construction period is anticipated to be short given the nature of the works and a construction environmental management would be secured via condition to reduce any potential impacts upon neighbours.

A number of submissions by local residents have raised concerns about noise and disturbance as a result of the existing care home at 8-9 Belvedere Road.

The proposed change of use would result in an increase in occupancy of 7 Belvedere Road from 8 to 14 bedrooms. Given this increase in occupancy, it is likely that the proposed change of use would result in a minor increase in noise. It should be noted that 7 Belvedere Road is detached from 6 Belvedere Road and that whilst the level of occupancy may have increased, officers consider that the levels of noise are not likely to be demonstrably different to existing.

The levels of servicing and deliveries outlined within the application are unlikely to result in a significant increase in noise and disturbance when compared to existing levels.

It is considered that the proposed change of use would not result in any unacceptable harm to existing residential amenity.

The proposed development would create 17 no. new bedrooms for residents in need of care.

Each of the proposed rooms would benefit from having a window and an en-suite bathroom.

The garden to the rear of the building would be retained and provide amenity space for residents, as well as the existing garden to the rear of 8-9 Belvedere Road.

Each of the rooms would exceed the requirements for a double-bedroom as set out in the Nationally Described Space Standards; with bedroom sizes ranging from a minimum of 13sqm up to 22sqm (in excess of the minimum 11.5sqm standard).

It is considered that the proposed development would offer sufficient space for future occupiers.



**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

In conclusion, the proposed development is considered to be acceptable in terms of residential amenity.

**F. HAS SUFFICIENT CONSIDERATION BEEN GIVEN TO SUSTAINABLE DESIGN AND CONSTRUCTION?**

Policy BCS13 sets out that development should contribute to both mitigating and adapting to climate change, and to meeting targets to reduce carbon dioxide emissions.

Policy BCS14 sets out that development in Bristol should include measures to reduce carbon dioxide emissions from energy use by minimising energy requirements, incorporating renewable energy sources and low-energy carbon sources. Development will be expected to provide sufficient renewable energy generation to reduce carbon dioxide emissions from residual energy use in the buildings by at least 20%.

The proposed development would include the provision of an air source heat pump to provide heating to the building. The Energy Strategy and Sustainability Statement submitted with the application sets out that the proposed development would comply with Part L of the Building Regulations and the inclusion of a heat hierarchy-compliant air source heat pump would achieve an energy saving of approximately 22%. This would accord with Policy BCS14.

In conclusion, it is considered that sufficient consideration has been given the sustainable design and construction.

**G. EQUALITIES ASSESSMENT**

During the determination of this application due regard has been given to the impact of this scheme in relation to the Equalities Act 2010 in terms of its impact upon key equalities protected characteristics. These characteristics are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. There is no indication or evidence (including from consultation with relevant groups) that different groups have or would have different needs, experiences, issues and priorities in relation this particular proposed development. Overall, it is considered that the approval of this application would not have any significant adverse impact upon different groups or implications for the Equalities Act 2010.

**CONCLUSION**

The proposed development is considered justified in principle due to the demands for care home beds and the proposed development would not reduce the choice of homes in the area sufficiently to warrant refusal.

The proposed external changes to the building are minor in nature and there is a benefit to the appearance of the terrace via the proposed removal of the door in the front elevation and replacement with a sash window to match the adjacent windows.

There would be some increase in noise associated with an increase in occupancy of the proposed development, however this is not considered by officers to be significant enough to warrant refusal given the retention of what is largely a residential use. The proposed extension would provide quality accommodation for future occupants.

The proposed development would provide Air Source Heat Pumps and as such, would be in accordance with the heat hierarchy and would provide a sufficient reduction in CO2 emissions.

The concerns of local residents are understood and the stresses on parking evident within the vicinity of the site. These issues are reiterated in the appeal decision and the parking surveys submitted. It is

**Development Control Committee B – 9 June 2021****Application No. 20/06030/F : 7 Belvedere Road Bristol BS6 7JG**

likely that the proposed change of use would cause further congestion and conflict, exacerbated by two-way working and would create a hazard for all road users.

It is recommended that the application is refused on the basis of the impact upon highway safety resulting from a lack of available parking in the area.

**RECOMMENDED      REFUSED**

1. The proposed development would result in an increased demand for on-street car parking in an area which is oversubscribed. This would lead congestion and conflict between road users and would result in harm to highway safety, contrary to Policies BCS10 (Transport and Access Improvements) and DM23 (Transport Development Management).

commdelgranted  
V1.0211

## **Supporting Documents**

### **1. 7 Belvedere Road**

1. Appeal decision for previous scheme
2. Existing Lower Ground Floor
3. Existing Upper Ground Floor
4. Existing First Floor
5. Existing Second Floor
6. Proposed Lower Ground Floor
7. Proposed Upper Ground Floor
8. Proposed First Floor
9. Proposed Second Floor
10. Proposed Front elevation
11. Applicant Parking Survey
12. WPCA Parking Survey

---

## Appeal Decision

Site visit made on 23 February 2021

**by J P Longmuir BA (Hons) DipUD MRTPI**

**an Inspector appointed by the Secretary of State**

**Decision date: 10 March 2021**

---

**Appeal Ref: APP/Z0116/W/20/3263935**

**7 Belvedere Road, Westbury Park, Bristol BS6 7JG**

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
  - The appeal is made by Mr Baryah on behalf of Meadowcare Homes against the decision of Bristol City Council.
  - The application Ref 19/03104/F, dated 20 June 2019, was refused by notice dated 2 June 2020.
  - The development proposed is change of use from 3 x flats to a 17 x bed extension to the nursing home at 8-9 Belvedere Road.
- 

### Decision

1. The appeal is dismissed.

### Main Issues

2. The main issues are:
  - the effects of the proposal on parking and highway safety and;
  - the effects of the proposal on the character and appearance of the area, including the Conservation Area, noise, disturbance, recycling/refuse and concentration of residential institutions/care homes.

### Reasons

#### *Highway safety*

3. Belvedere Road is in a suburb of the city, close to mainstream facilities and services. It is predominately a residential street with several existing care homes.
4. Belvedere Road is part of a network of side roads, off the main direct thoroughfare. There is unrestricted parking along both sides of the road and two way traffic. Few of the dwellings have their own off street parking spaces.
5. The comments and photographs from local residents indicate a significant shortfall in parking. The appellants have undertaken their own surveys.
6. I undertook my site visit at 7-05 a.m. and I noted only two spaces available on the whole street. The parking would appear to be most severe during non-working times indicating that this is a problem for residents and is not caused by incoming commuters.

7. The parking surveys are only a snapshot in time and do not necessarily represent the situation at all times. Nonetheless it is clear to me that the parking is at a premium. As I saw several times on my site visit, the parking situation results in vehicles often having to park in the middle road. This causes congestion and conflict which is exacerbated by two way working and creates a hazard for all road users.
8. The existing building on the appeal site is subdivided into three flats. The proposal involves the loss of these three flats and the conversion of the building into an extension for the neighbouring care home. The removal of these 3 flats would ease parking demand as the care home residents would not be car owners. I note the submissions show the typical parking in the area for flats: 26% no car, 49% 1 car, 25% 2 or more. Assuming an average of one space per dwelling, the removal of the flats would ease demand potentially by 3 spaces.
9. The proposal would lead to two additional staff, from 08:00 to 14:00 and thereafter one additional staff member. The submission indicates that this would equate to the need for one additional parking space.
10. However, the proposal would lead to increased visitors which would be more likely to be significant at weekends. Although some of these visitors may arrive by public transport, this would not wholly be the case.
11. The proposal would also lead to additional deliveries and servicing. This would lead to more pressure for indiscriminate parking.
12. The submitted Transport Statement (TS) includes the provision of bays for ambulances outside the care homes at No. 2-3 and 8-9 Belvedere Road. However, this would result in the loss of parking spaces, where they are in great demand.
13. The suggested bays would help the emergency services and provide dignity and reassurance for patients. However, these bays may also be needed for deliveries and/or passing bays otherwise their value would be very limited, particularly with the conflicts of two way traffic. Therefore, the demarcation, specification and role of these bays would therefore need to be clarified.
14. These bays must function to optimum benefit to compensate for the loss of the on street parking. However, I do not find that there is sufficient clarity of how they would function and a planning condition would have to be specific in its requirements.
15. A Traffic Regulation Order (TRO) would also be required for the measures. The order making process requires consultation with residents and other parties. There is no certainty what conclusion would result. The loss of parking would have to be considered as well as the safety implications for pedestrians and motorists.
16. Whilst the suggested condition is worded to prevent a commencement pending the TRO, such a situation would lead to uncertainty.
17. Based on the submitted evidence, the TRO has uncertain deliverability and its overall benefits have not been demonstrated against the loss of car parking.

18. I therefore conclude that the proposal as submitted would harm highway safety. Policies BCS10, DM2 and DM23 of the Bristol Local Plan Site Allocations and Development Management Policies (LP) require safe streets integrated with the development, the regulation of parking impacts from shared housing and the avoidance of unacceptable traffic conditions. Paragraph 110 of the National Planning Policy Framework (The Framework) highlights the needs for deliveries and emergency vehicles and safety of streets for all users. Paragraph 109 of the Framework states that development should only be prevented if there would be an unacceptable impact on highway safety, or the residual cumulative impacts would be severe. The proposal would be in conflict with these policies.

*Character of the area, conservation area and noise/disturbance*

19. The appeal site is on the south side of Belvedere Road and is an integral part of a row of tall buildings. These have a substantial presence and look overly large for family dwellings and the use as a care home would be expected and accepted. Consequently, the use would not appear inappropriate.
20. I note concern about the concentration of care homes. This would be an extension and would be connected by the side rather than making a new frontage and entrance, which would have highlighted the use. Moreover, the proposal involves the removal of three flats which do not contribute to the perception of the area for established family houses. On the opposite side of the road are two storey detached houses which appear overtly different and would be perceived as family houses. I therefore conclude that this particular proposal would not lead to the harmful perception of excessive care homes.
21. Noise and disturbance were part of the reason for refusal in the decision notice. The proposal would remove 3 flats and as the care home is managed these issues would be reduced. There would be less pedestrian movements and therefore less potential noise/disturbance at any time of day or night. I note the comments about the distressing sight of ambulances being loaded, however such situations could happen anywhere.
22. The proposal involves the removal of the frontage door and changes to the windows. This would restore the window to its original form which would help the symmetrical appearance of the building. There would also be a dormer to the side roof, which is modest and sympathetic with the roof form. The rear dormer is similar to that on No. 8. These alterations would not detract from the form and detailing of the building which is orientated towards the front. There would be some lightwells for basement conversion, but these would be inconsequential.
23. As I observed on my site visit the 3 flats have bins spread over a dispersed area on the frontage. The proposal would allow scope for rationalising the bin storage in conjunction with the adjacent building. This would offset the extra volume of waste/recycling. Cycle parking would be provided to the front but would not be detracting. There would also be potential for landscaping.
24. The appeal site is within the Downs Conservation Area. This is notable for the consistency of the stone buildings with rich detailing, the formality of the layout of the streets and the street trees. The specialist officers at the Council had no objection in this regard. I concur bearing in mind the implications of the intended use as well as the physical alterations.

25. I therefore conclude that the proposal would not harm the character and appearance of the area including the conservation area and would not lead to significant noise and disturbance.
26. Policies BCS18 and DM2 of the LP seek to avoid an over concentration of residential institutions but do not quantify the amount. They also highlight excessive noise/disturbance, and problems of waste storage. The proposal would not be in conflict with these criteria and notably DM2 encourages accommodation for older persons in areas close to shops, facilities and public transport. Furthermore paragraph 61 of the Framework emphasises the need of providing housing for different groups in the community. Paragraph 8 has a social objective of providing a range of homes with accessible services. Paragraph 193 of the Framework requires great weight is given to the assets conservation and the proposal would not be in conflict.

### **Planning Balance**

27. The proposal would provide specialist care by an established practice. There is a need for the proposal, particularly with an ageing population. Care is best provided within the community and a sustainable location. However, the proposal as currently submitted has not addressed the highway safety implications.

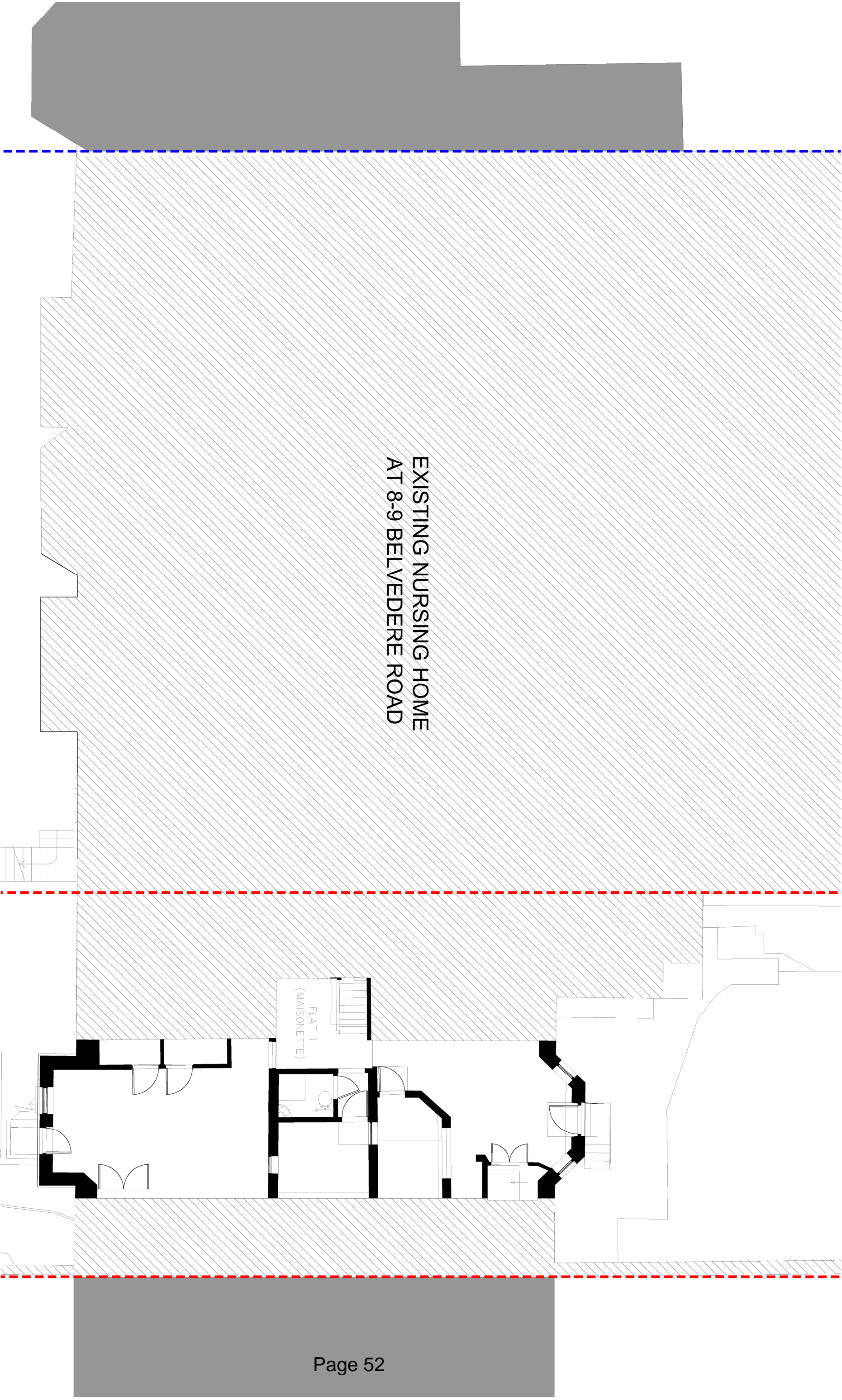
### **Conclusion**

28. I therefore conclude that the appeal should be dismissed.

*John Longmuir*

INSPECTOR





EXISTING NURSING HOME  
AT 8-9 BELVEDERE ROAD

FLAT 1  
(MAISONNETTE)

LOWER GROUND



revisions

project

**Care-Home**  
7, BELVEDERE ROAD  
scale  
1:100 @ A3

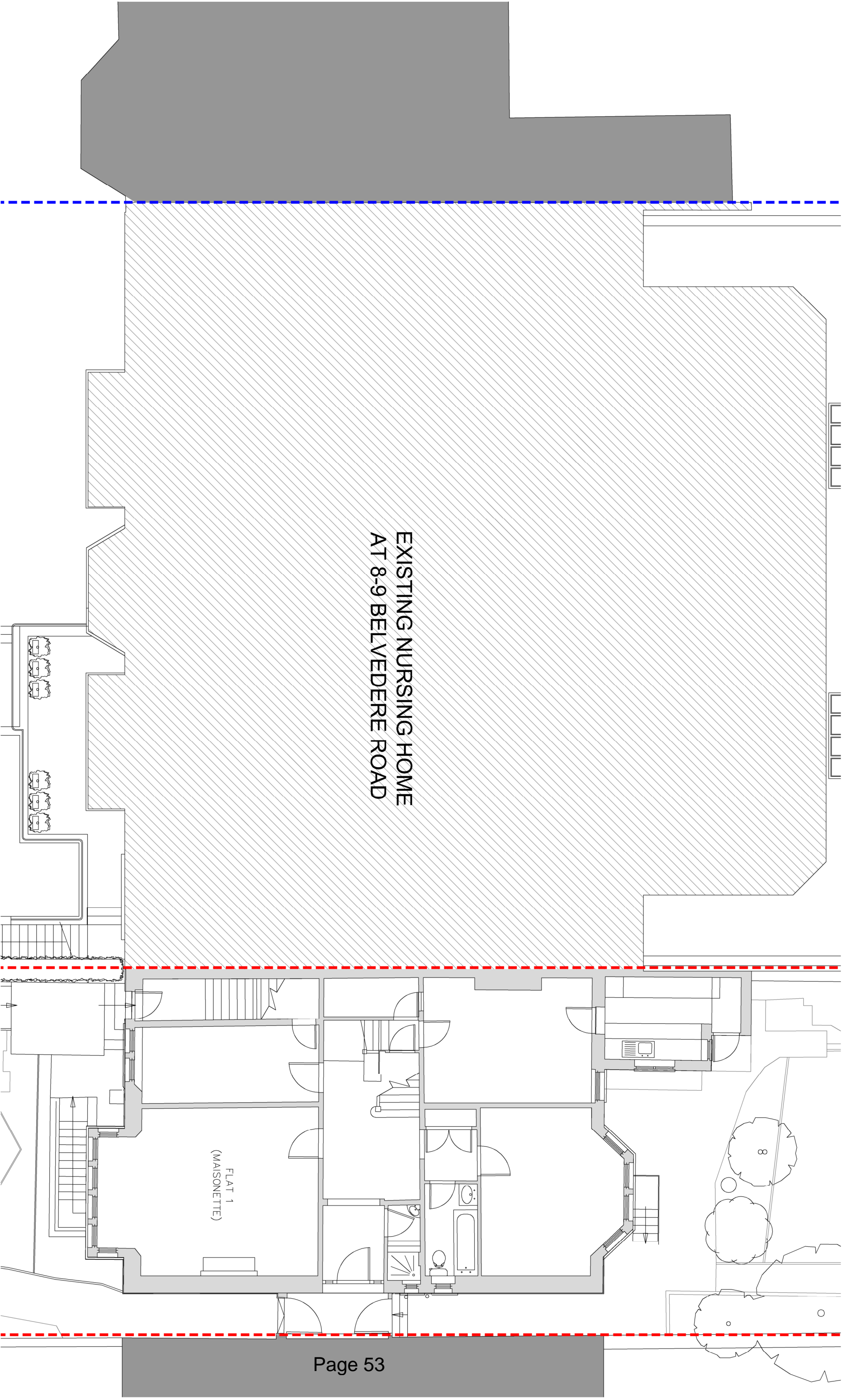
drawing

**PLAN:FLOORS**  
EXISTING  
drawn  
I.N

drawing number

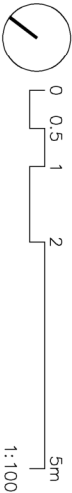
**033:S:01**  
date  
NOV 2020





EXISTING NURSING HOME  
AT 8-9 BELVEDERE ROAD

FLAT 1  
(MAISONETTE)



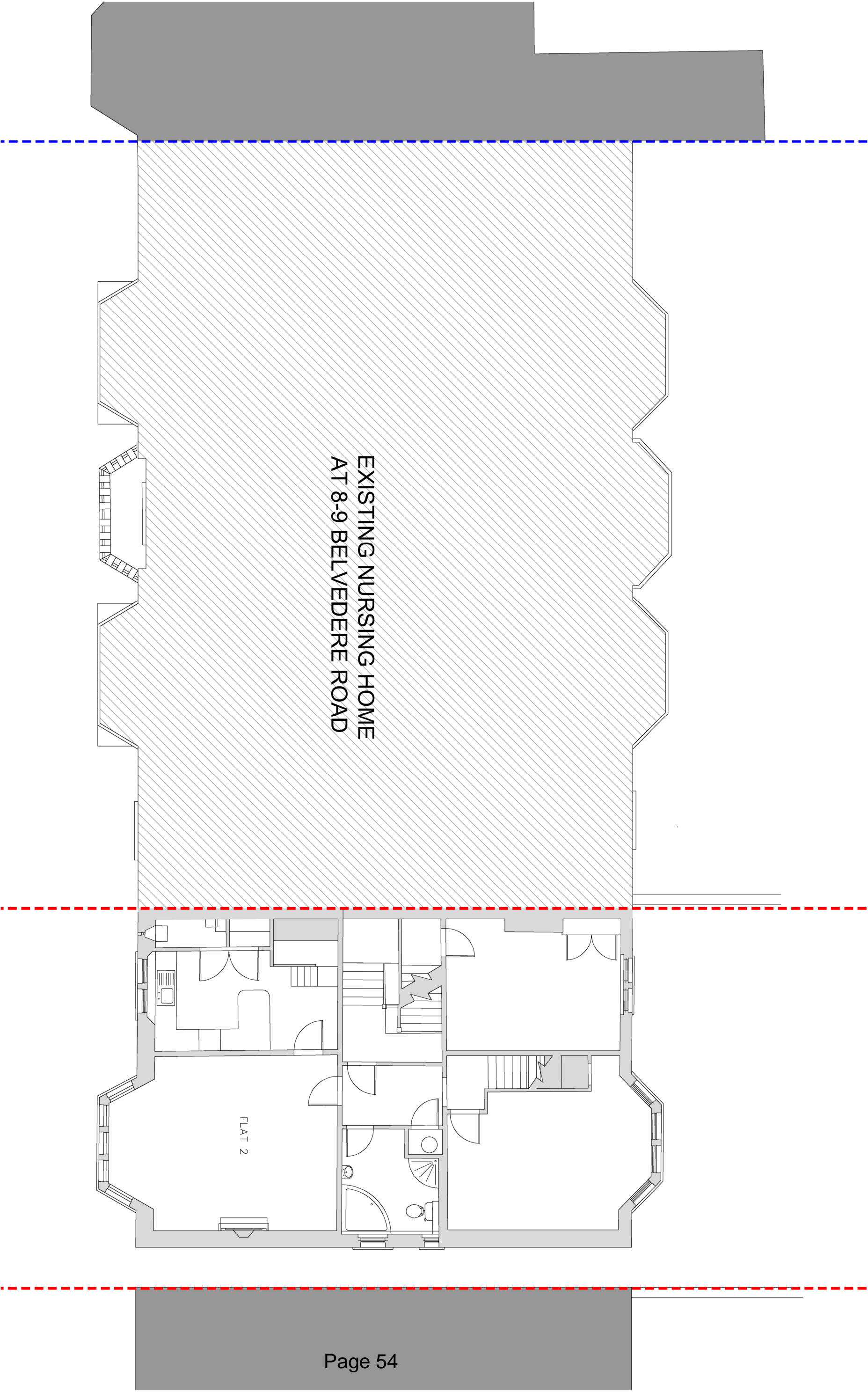
UPPER GROUND

revisions

project

drawing

drawing number



FIRST FLOOR

drawing number

033:S:03

date

NOV 2020

drawing

PLAN:FLOORS  
EXISTING

drawn

I.N

project

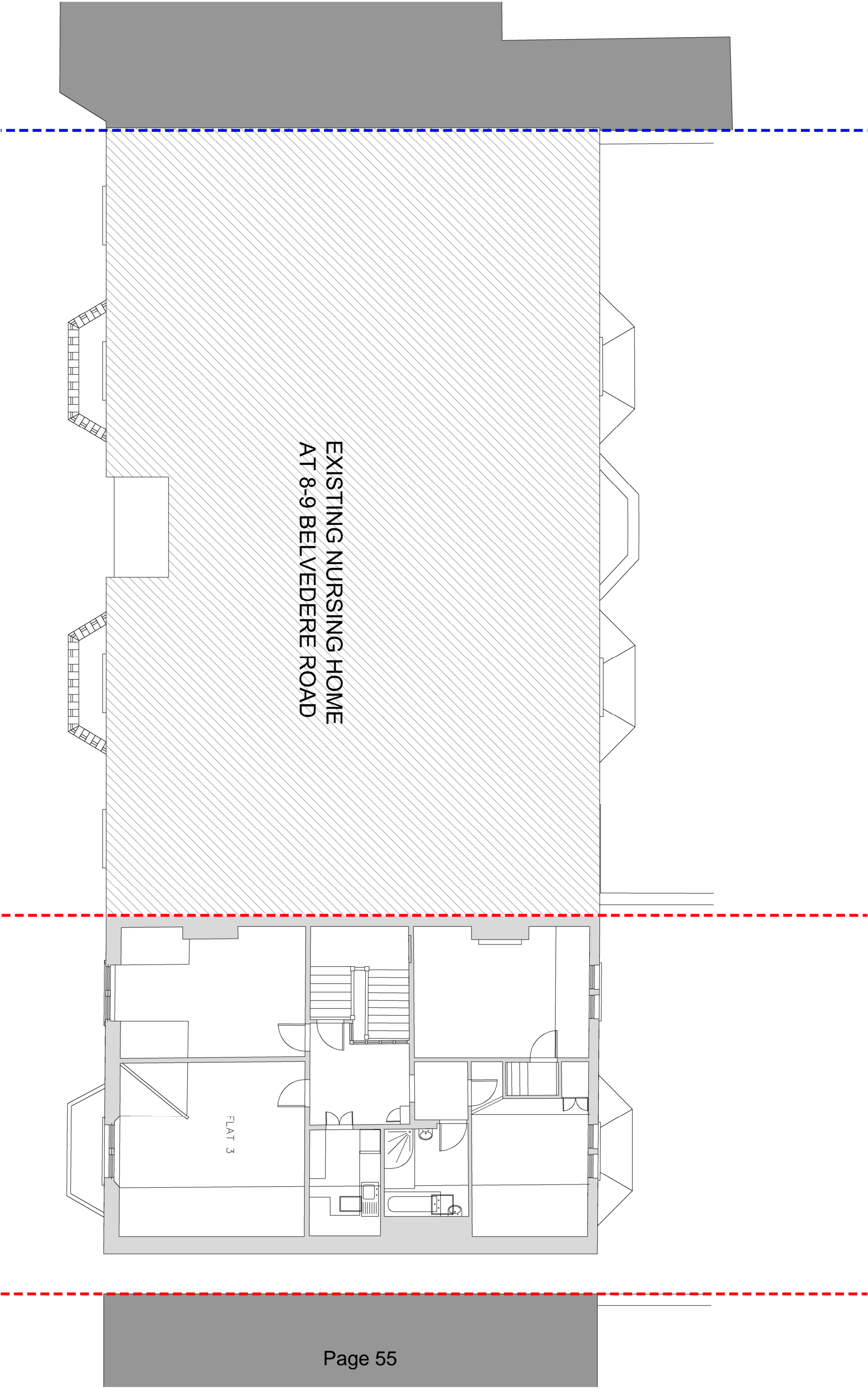
Care-Home  
7, BELVEDERE ROAD

scale

1:100 @ A3

revisions





SECOND FLOOR

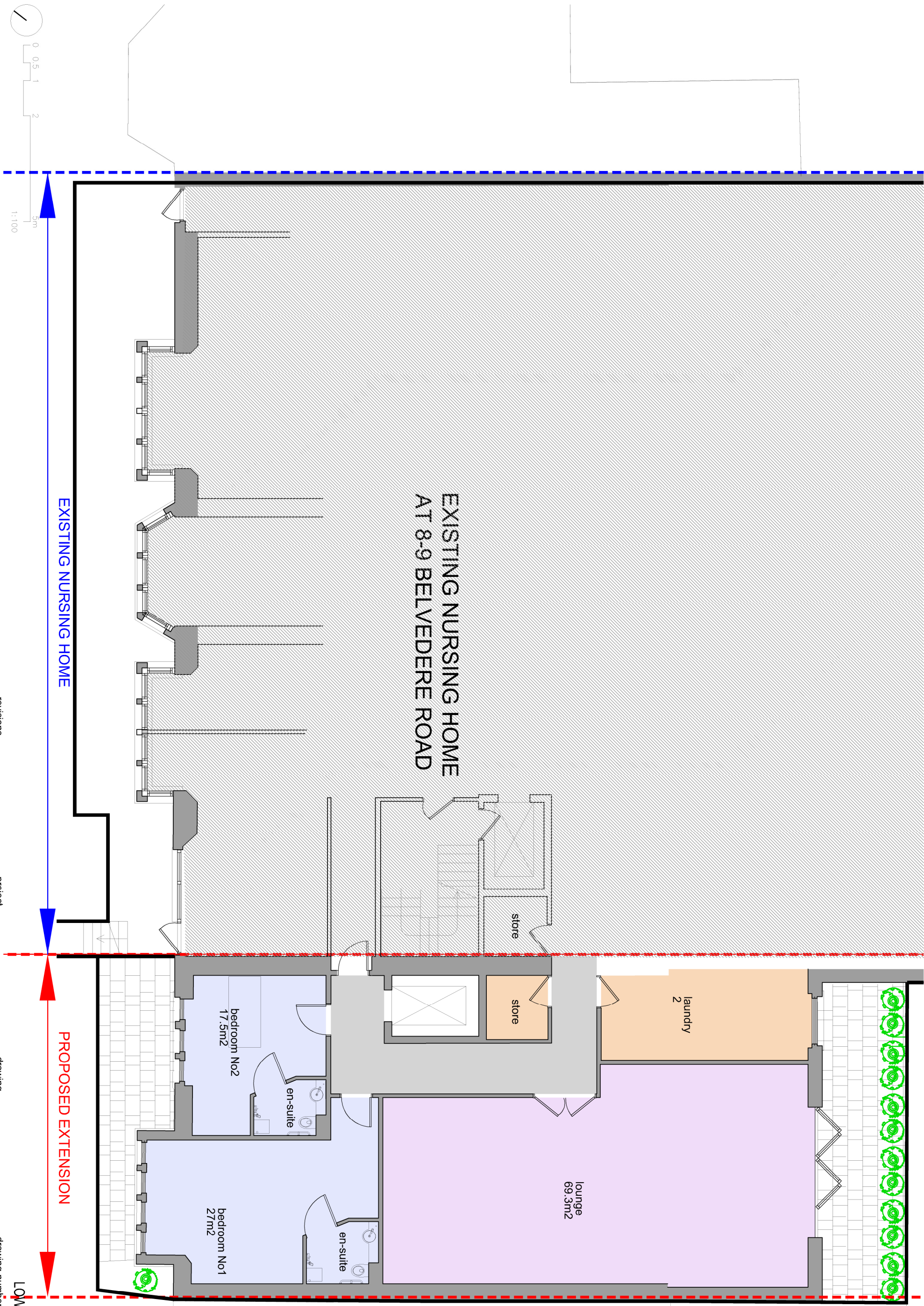
revisions

project

drawing

drawing number





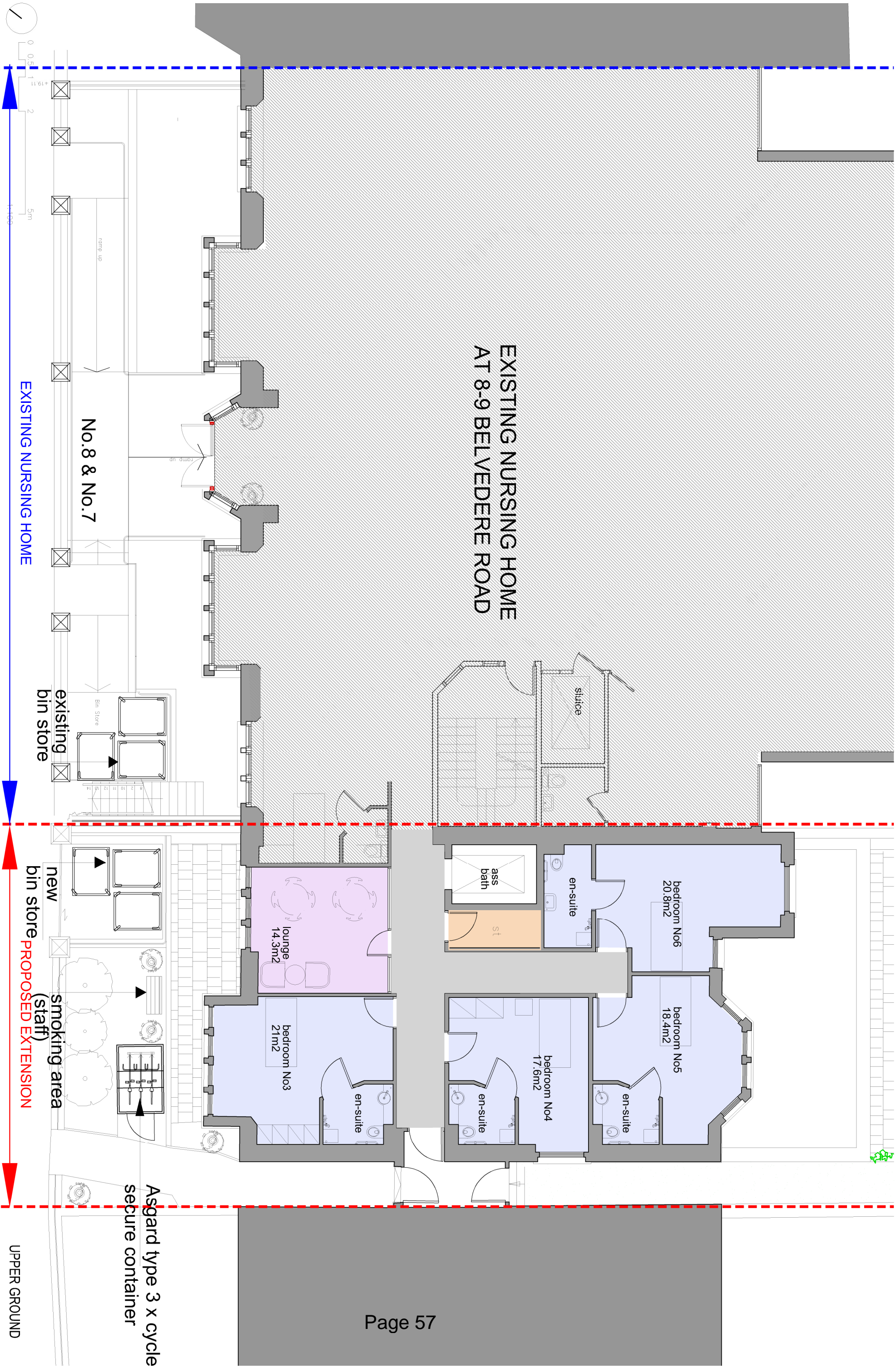
EXISTING NURSING HOME  
AT 8-9 BELVEDERE ROAD

EXISTING NURSING HOME

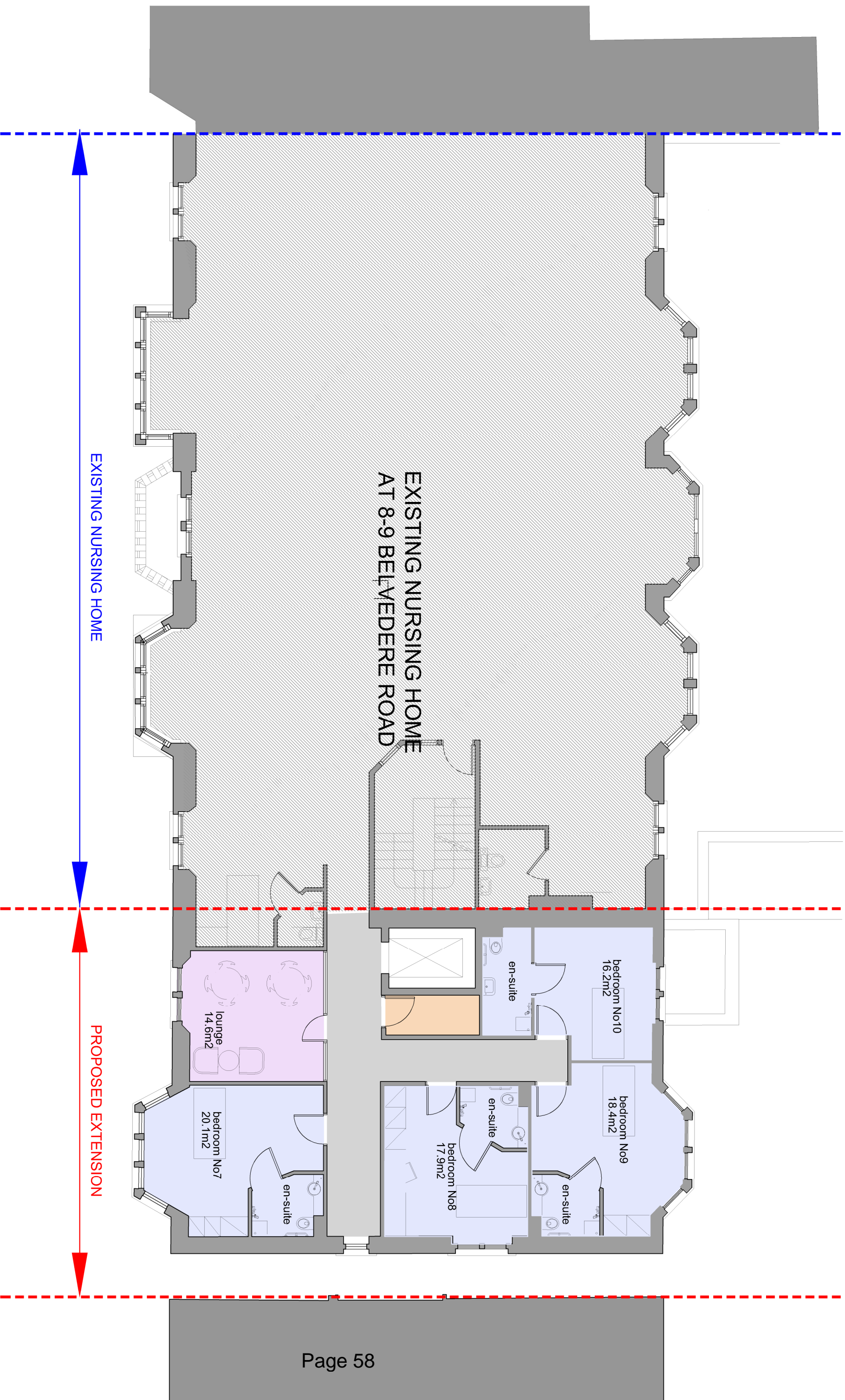
PROPOSED EXTENSION

LOWER GROUND

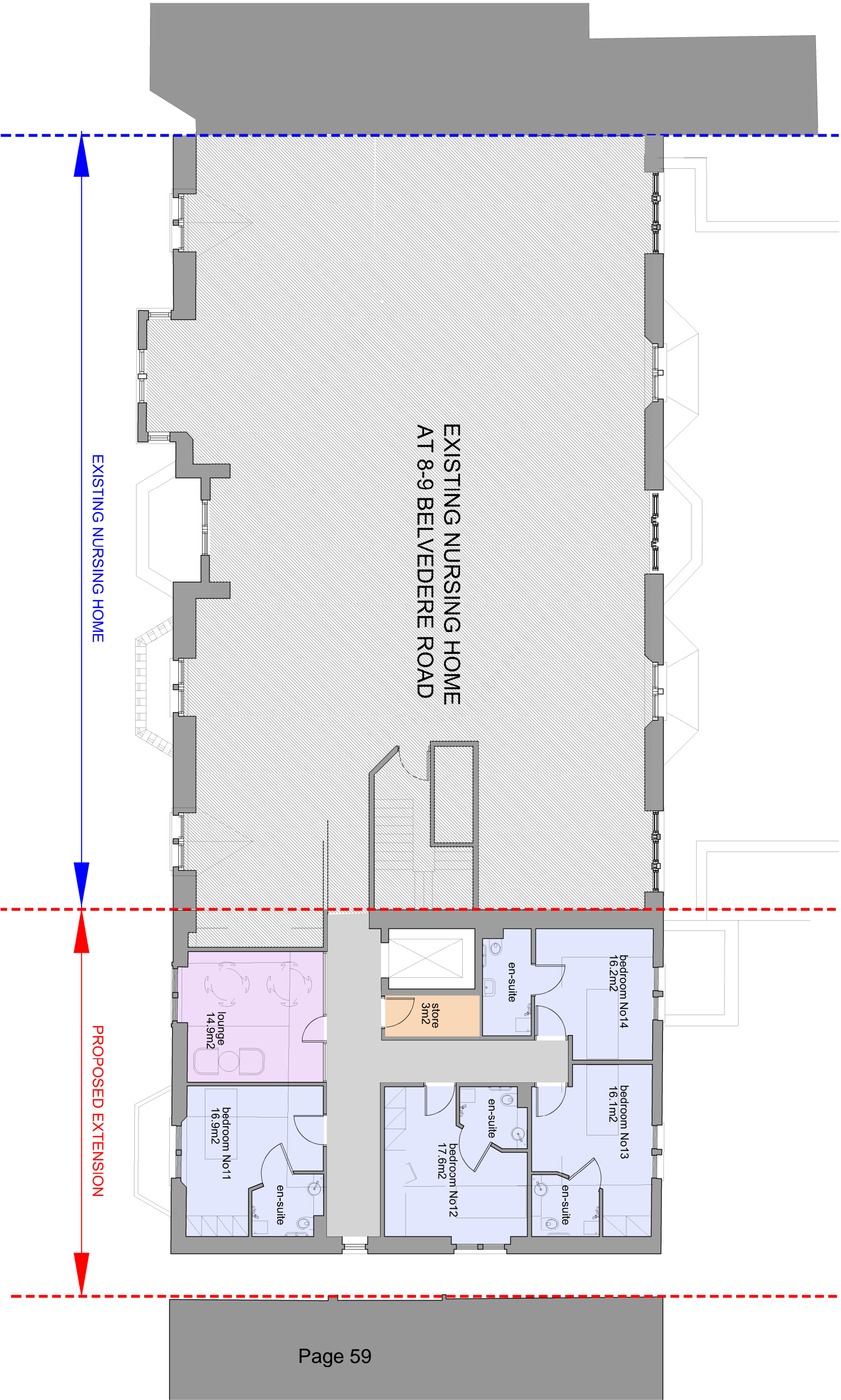












SECOND

revisions

project

Care-Home  
7, BELVEDERE ROAD  
scale  
1:100 @ A3

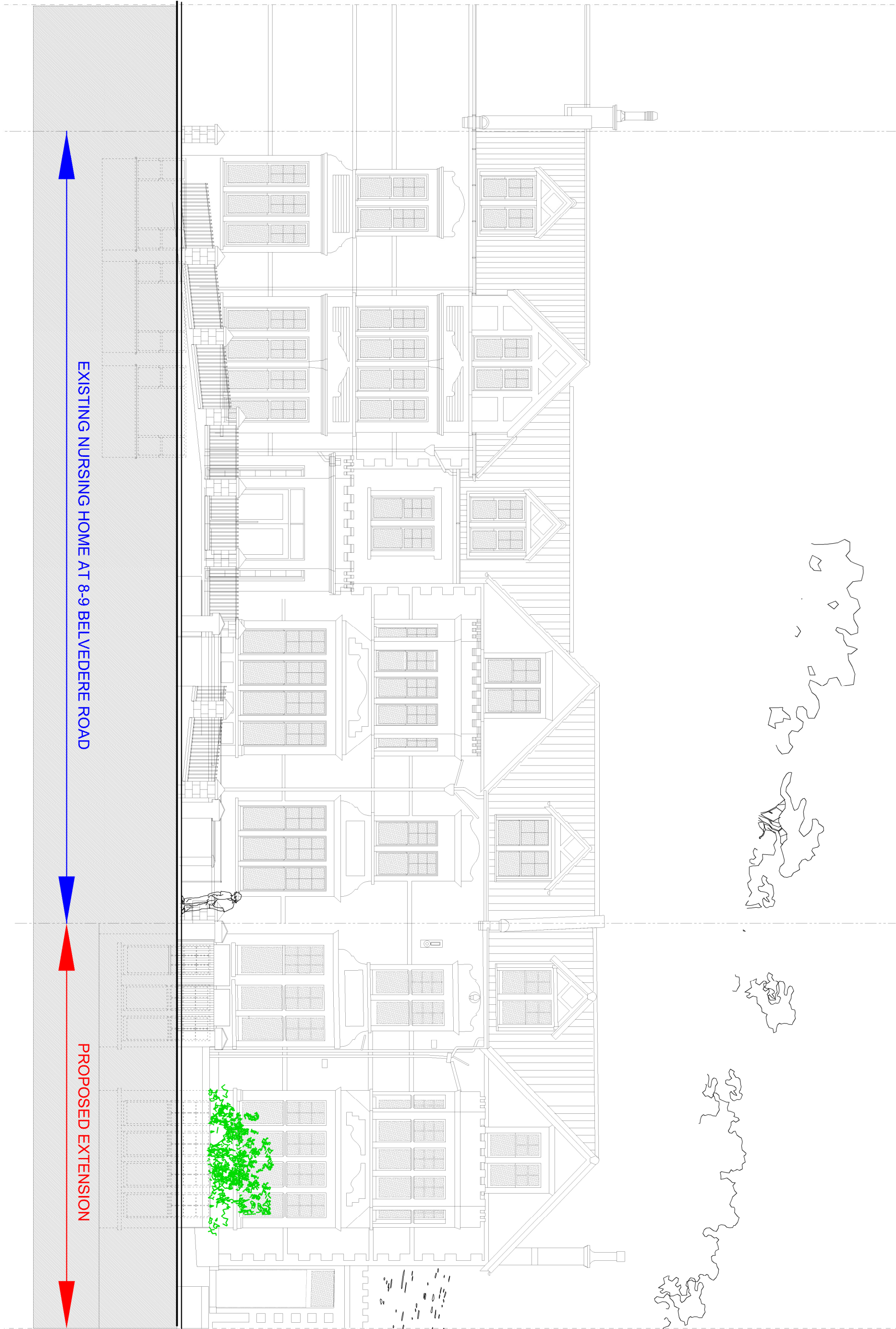
drawing

PLAN:FLOORS  
PROPOSED  
drawn  
I.N.

drawing number

033:005  
date  
NOV 2020





Belvedere Road Elevation

revisions

project

drawing

drawing number

**Hadfield+Noblin**  
Architects

The Old Brushworks, 56 Pickwick Road,  
Corsham, Wiltshire, SN13 9BX  
david@hadfield-noblin.com  
ines@hadfield-noblin.com  
Tel : 01249 714229

**Care-Home**  
7, BELVEDERE ROAD

scale  
1:100 @ A3

**ELEVATION:NORTH**  
PROPOSED

drawn  
I.N.

date  
NOV 2020

**033:006**



# Parking Beat Survey

7 Belvedere Road, Bristol



Wednesday 28th October 2020



Created by Bert Ramos

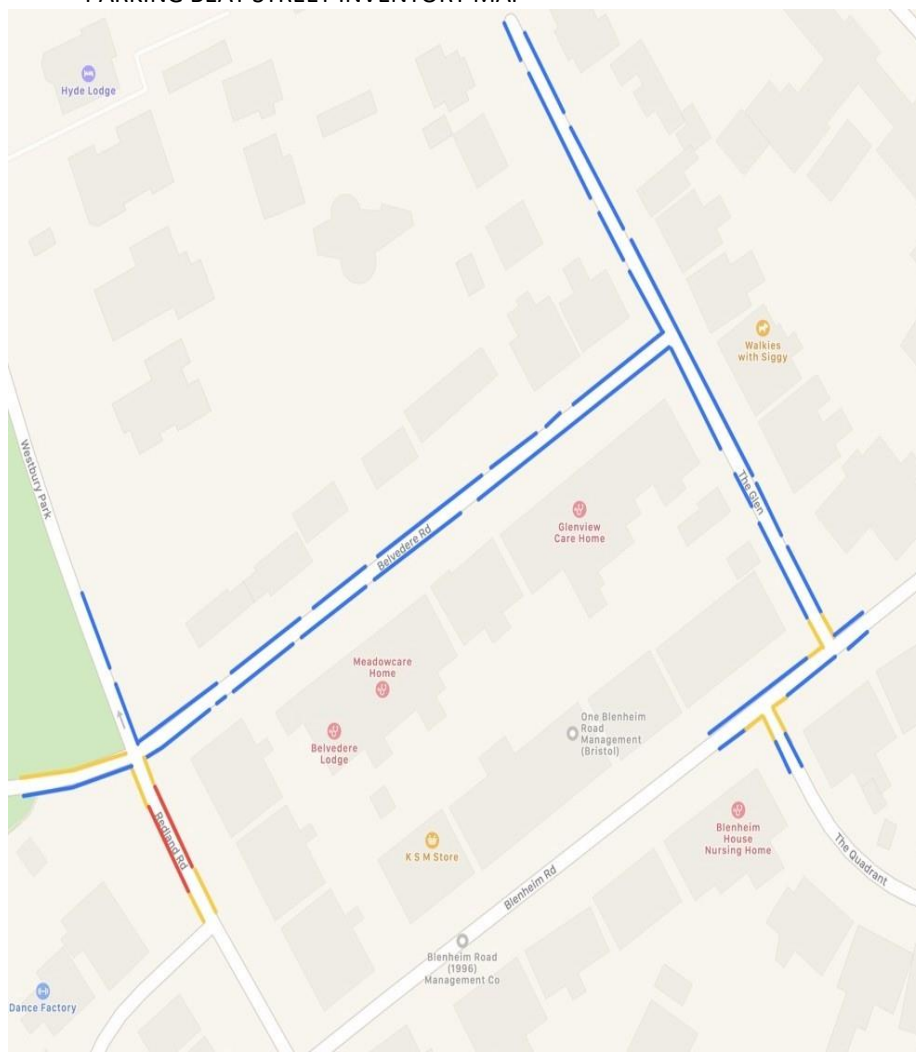
## SURVEY DETAILS

<b>Survey Type</b>	PARKING BEAT SURVEY
<b>Methodology Guidance</b>	London Borough of Lambeth
<b>Site</b>	7 Belvedere Road, Bristol
<b>Survey Area</b>	150 metre walking distance from site
<b>Date/s</b>	Wednesday 28th October 2020
<b>Time/s</b>	07:00, 08:00, 13:30, 14:30, 19:30, 20:30
<b>Beat Frequency</b>	Snapshot
<b>Unit for 1 Unmarked Lengthwise Space (m)</b>	5
<b>Unit for 1 Unmarked Crosswise Space (m)</b>	2.5
<b>Areas Excluded From Survey</b>	Private parking spaces, private roads and off road parking (unless requested in survey specification).
<b>Sections of road excluded from parking capacity calculation</b>	<p>First 7.5m from junction mouth (for reasons of highway safety).</p> <p>Crossovers, dropped kerbs, build-outs, traffic islands, 24/7 illegal parking.</p> <p>Sections of legal lengthwise parking between illegal parking (crossover, dropped kerbs, double yellow etc) that measure less than the unit specified for 1 space.</p> <p>Where the width of the road is such that parking on both sides would cause an obstruction. In this instance one side of the road has been excluded from the capacity calculation.</p>
<b>Parking excluded from stress calculation</b>	<p>Skips or any other non-vehicle occupying a parking space (but noted separately if observed).</p> <p>Any illegal parking on double yellow lines, crossovers, keep clear lines etc (but noted separately if observed).</p>
<b>Terminology</b>	<p>"Parking Stress" - Calculation to express the number of parked vehicles as a percentage of available parking for each parking type. Stress can be over 100% if cars are small and/or parked very closely together.</p> <p>"Parking Capacity Calculation" - Measurement of each length of road between illegal parking (e.g. crossovers, traffic islands, double yellow etc) converted into parking spaces by rounding down to the nearest unit assigned to one parking space and dividing this figure by the unit.</p> <p>"Lengthwise Parking" - Vehicles parked in a lengthwise orientation with wheels parallel to the kerbside.</p> <p>"Crosswise Parking" - Vehicles parked in a crosswise orientation (as seen in car parks or wide sections of road)</p>

## SITE PLAN

7 Belvedere Road, Bristol

### PARKING BEAT STREET INVENTORY MAP



KEY:

Double Yellow Lines

Disabled Parking

Unrestricted

Permit Holder Parking

Too Narrow/Crossover/Junction

# PARKING STRESS TABLES

Restriction				1	Unrestricted																	
Location	Lengthwise Parking (m)	Lengthwise Spaces	Marked/Crosswise Bays	Total Spaces	Wednesday 28th October 2020																	
					7:00			8:00			13:30			14:30			19:30			20:30		
					Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)
Belvedere Road	255	51	0	51	48	3	94%	46	5	90%	45	6	88%	46	5	90%	47	4	92%	45	6	88%
The Glen	210	42	0	42	38	4	90%	39	3	93%	38	4	90%	37	5	88%	33	9	79%	36	6	86%
Blenheim Road	75	15	0	15	14	1	93%	15	0	100%	14	1	93%	15	0	100%	13	2	87%	13	2	87%
The Quadrant	30	6	0	6	5	1	83%	5	1	83%	6	0	100%	5	1	83%	6	0	100%	6	0	100%
Westbury Park	40	8	0	8	6	2	75%	7	1	88%	7	1	88%	8	0	100%	8	0	100%	7	1	88%
Claypit Road	40	8	0	8	7	1	88%	8	0	100%	8	0	100%	7	1	88%	6	2	75%	7	1	88%
	0	0	0	0		0	0%		0	0%		0	0%		0	0%		0	0%		0	0%
Total	650	130	0	130	118	12	91%	120	10	92%	118	12	91%	118	12	91%	113	17	87%	114	16	88%

Restriction				2	Permit Holders (Mon-Fri 9am-5pm)																	
Location	Lengthwise Parking (m)	Lengthwise Spaces	Marked/Crosswise Bays	Total Spaces	Wednesday 28th October 2020																	
					7:00			8:00			13:30			14:30			19:30			20:30		
					Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)	Occupied	Spaces	Stress (%)
Redland Road	20	4	0	4	3	1	75%	4	0	100%	3	1	75%	2	2	50%	2	2	50%	3	1	75%
	0	0	0	0		0	0%		0	0%		0	0%		0	0%		0	0%		0	0%
Total	20	4	0	4	3	1	75%	4	0	100%	3	1	75%	2	2	50%	2	2	50%	3	1	75%

# Illegal/Obstructive Parking

Location	Description	Wednesday 28th October 2020					
		7:00		8:00		13:30	
		Occupied		Occupied		Occupied	
	Double Yellow	0		0		0	
	Double Yellow	0		0		0	
Total		0		0		0	

## PARKING CAPACITY MEASUREMENTS

A working table showing kerbside measurements for each parking type.

Location	Side of Road & Measuring Orientation	Parking Type	Section Length (m)	Crosswise Spaces or Lengthwise Marked Bays	Number of Crosswise Spaces or Marked Bays	Unit Round Down (If Lengthwise & Unmarked)	Total Spaces
Westbury Park	W N-S	Too Narrow to Park	45			45	9
Westbury Park	E S-N	Unrestricted	25.9			25	5
Westbury Park	E S-N	Crossover	5.9			5	1
Westbury Park	E S-N	Unrestricted	15			15	3
The Quadrant	E N-S	Double Yellow Lines	7.2			5	1
The Quadrant	E N-S	Unrestricted	15			15	3
The Quadrant	W S-N	Unrestricted	15.1			15	3
The Quadrant	W S-N	Double Yellow Lines	7.5			5	1
Redland Road	W N-S	Double Yellow Lines	7.2			5	1
Redland Road	W N-S	Permit Holders (Mon-Fri 9am-5pm)	10.2			10	2
Redland Road	W N-S	Double Yellow Lines	4.5			0	0
Redland Road	E S-N	Double Yellow Lines	18.1			15	3
Redland Road	E S-N	Permit Holders (Mon-Fri 9am-5pm)	10.1			10	2
Redland Road	E S-N	Double Yellow Lines	4.5			0	0
Claypit Road	N E-W	Double Yellow Lines	40			40	8
Claypit Road	S W-E	Unrestricted	40			40	8
Belvedere Road	N W-E	Unrestricted	39.7			35	7
Belvedere Road	N W-E	Crossover	6.8			5	1
Belvedere Road	N W-E	Unrestricted	30.1			30	6
Belvedere Road	N W-E	Crossover	6.3			5	1
Belvedere Road	N W-E	Unrestricted	17			15	3
Belvedere Road	N W-E	Crossover	4.5			0	0
Belvedere Road	N W-E	Unrestricted	20.1			20	4
Belvedere Road	N W-E	Crossover	9			5	1
Belvedere Road	N W-E	Unrestricted	8.6			5	1
Belvedere Road	N W-E	Crossover	4.3			0	0
Belvedere Road	N W-E	Unrestricted	1.9			0	0
Belvedere Road	N W-E	Crossover	4.7			0	0
Belvedere Road	N W-E	Unrestricted	11.9			10	2
Belvedere Road	S E-W	Unrestricted	40			40	8
Belvedere Road	S E-W	Crossover	4.4			0	0
Belvedere Road	S E-W	Unrestricted	17.6			15	3
Belvedere Road	S E-W	Crossover	4			0	0
Belvedere Road	S E-W	Unrestricted	28.4			25	5
Belvedere Road	S E-W	Crossover	2.8			0	0
Belvedere Road	S E-W	Unrestricted	25.3			25	5
Belvedere Road	S E-W	Crossover	2.7			0	0
Belvedere Road	S E-W	Unrestricted	2.2			0	0
Belvedere Road	S E-W	Crossover	3			0	0
Belvedere Road	S E-W	Unrestricted	35.4			35	7
The Glen	W S-N	Double Yellow Lines	5.7			5	1
The Glen	W S-N	Unrestricted	17.6			15	3

The Glen	W S-N	Crossover	8.1			5	1
The Glen	W S-N	Unrestricted	5.6			5	1
The Glen	W S-N	Crossover	9.1			5	1
The Glen	W S-N	Unrestricted	24.6			20	4
The Glen	W S-N	Junction	12.1			10	2
The Glen	W S-N	Unrestricted	15.8			15	3
The Glen	W S-N	Crossover	4.6			0	0
The Glen	W S-N	Unrestricted	9.7			5	1
The Glen	W S-N	Crossover	3.1			0	0
The Glen	W S-N	Unrestricted	29.2			25	5
The Glen	W S-N	Crossover	6.9			5	1
The Glen	W S-N	Unrestricted	6.5			5	1
The Glen	E N-S	Crossover	2.9			0	0
The Glen	E N-S	Unrestricted	13.5			10	2
The Glen	E N-S	Crossover	5.1			5	1
The Glen	E N-S	Unrestricted	22.3			20	4
The Glen	E N-S	Crossover	4.6			0	0
The Glen	E N-S	Unrestricted	60.7			60	12
The Glen	E N-S	Crossover	3.3			0	0
The Glen	E N-S	Unrestricted	16.4			15	3
The Glen	E N-S	Crossover	6.1			5	1
The Glen	E N-S	Unrestricted	18.3			15	3
The Glen	E N-S	Double Yellow Lines	6.5			5	1
Blenheim Road	N W-E	Unrestricted	35			35	7
Blenheim Road	N W-E	Double Yellow Lines	2.4			0	0
Blenheim Road	N W-E	Junction	13.4			10	2
Blenheim Road	N W-E	Unrestricted	15			15	3
Blenheim Road	S E-W	Unrestricted	5			5	1
Blenheim Road	S E-W	Crossover	3.9			0	0
Blenheim Road	S E-W	Unrestricted	16.6			15	3
Blenheim Road	S E-W	Double Yellow Lines	3.2			0	0
Blenheim Road	S E-W	Junction	13.3			10	2
Blenheim Road	S E-W	Double Yellow Lines	1.9			0	0
Blenheim Road	S E-W	Unrestricted	7.3			5	1
Blenheim Road	S E-W	Crossover	7.4			5	1

**BELVEDERE ROAD AREA PARKING SURVEY CONDUCTED BY WESTBURY PARK  
COMMUNITY ASSOCIATION, JANUARY 2021**  
UNRESTRICTED SPACES WITHIN A 150 METRE WALKING DISTANCE OF 7 BELVEDERE ROAD  
(AS PER THE ENTRAN PARKING BEAT SURVEY)

Mon, 11 <sup>th</sup> Jan.		12.05 pm			3.45 pm			7.00 pm		
Road	Total Potential Spaces	Full Spaces Occ.	Full Spaces Vacant	% Full Spaces Occ.	Full Spaces Occ.	Full Spaces Vacant	% Full Spaces Occ.	Full Spac Occ.	Full Spaces Vacant	% Full Spaces Occ.
Belvedere Road	48	48	0	100%	48	0	100%	47	0	100%
The Glen	45	44	0	100%	43	2	96%	42	3	93%
Blenheim Road	15	14	1	93%	14	1	93%	14	1	93%
The Quadrant	6	6	0	100%	6	0	100%	6	0	100 %
Westbury Park	8	8	0	100%	8	0	100%	7	1	87%
Claypit Road	8	8	0	100%	8	0	100%	5	3	62%
<b>Total</b>	130	128	1	99%	127	3	98%	121	8	94%

Tue, 12 <sup>th</sup> Jan.		10.45 am			3.00 pm			7.30 pm		
Road	Total Potential Spaces	Full Spaces Occ.	Full Spaces Vacant	% Full Spaces Occ.	Full Spaces Occ.	Full Spaces Vacant	% Full Spaces Occ.	Full Sp. Occ.	Full Spaces Vacant	% Full Spaces Occ.
Belvedere Road	48	46	0	100%	47	0	100%	47	0	100%
The Glen	45	43	2	96%	43	2	96%	40	4	91%
Blenheim Road	15	15	0	100%	15	0	100%	14	1	93%
The Quadrant	6	6	0	100%	6	0	100%	6	0	100%
Westbury Park	8	8	0	100%	8	0	100%	7	1	87%
Claypit Road	8	8	0	100%	8	0	100%	6	2	75%
<b>Total</b>	130	126	2	98%	127	2	98%	120	8	94%

Wed, 13 <sup>th</sup> Jan.		11.00 am			3.20 pm			7.45 pm		
Road	Total Potential Spaces	Full Spaces Occ.	Full Spaces Vacant	% Full Spaces Occ.	Full Spaces Occ.	Full Spaces Vacant	% Full Spaces Occ.	Full Sp Occ.	Full Spaces Vacant	% Full Spaces Occ.
Belvedere Road	48	47	1	98%	42	5*	89%	44	0 (4**)	100%
The Glen	45	45	0	100%	45	0	100%	43	2	96%
Blenheim Road	15	15	0	100%	14	1	93%	14	1	93%
The Quadrant	6	6	0	100%	6	0	100%	5	1	83%
Westbury Park	8	7	1	87%	8	0	100%	8	0	100%
Claypit Road	8	8	0	100%	6	2	75%	7	1	87%
<b>Total</b>	130	128	2	98%	121	8	94%	121	5	96%

\* Possibly school run time

\* \* Four spaces were vacant but these had been cordoned off pending tree works to be carried out by the City Council on Thursday 14<sup>th</sup> January. These spaces were therefore not available for parking.

## Development Control Committee B – 9 June 2021

ITEM NO. 2

**WARD:** Windmill Hill

**SITE ADDRESS:** St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS

**APPLICATION NO:** 20/04678/F Full Planning

**DETERMINATION DEADLINE:** 18 June 2021

**Redevelopment of existing site with demolition of existing NHS healthcare facility and William Hill building (no. 217) to create an affordable housing complex, (36 units).**

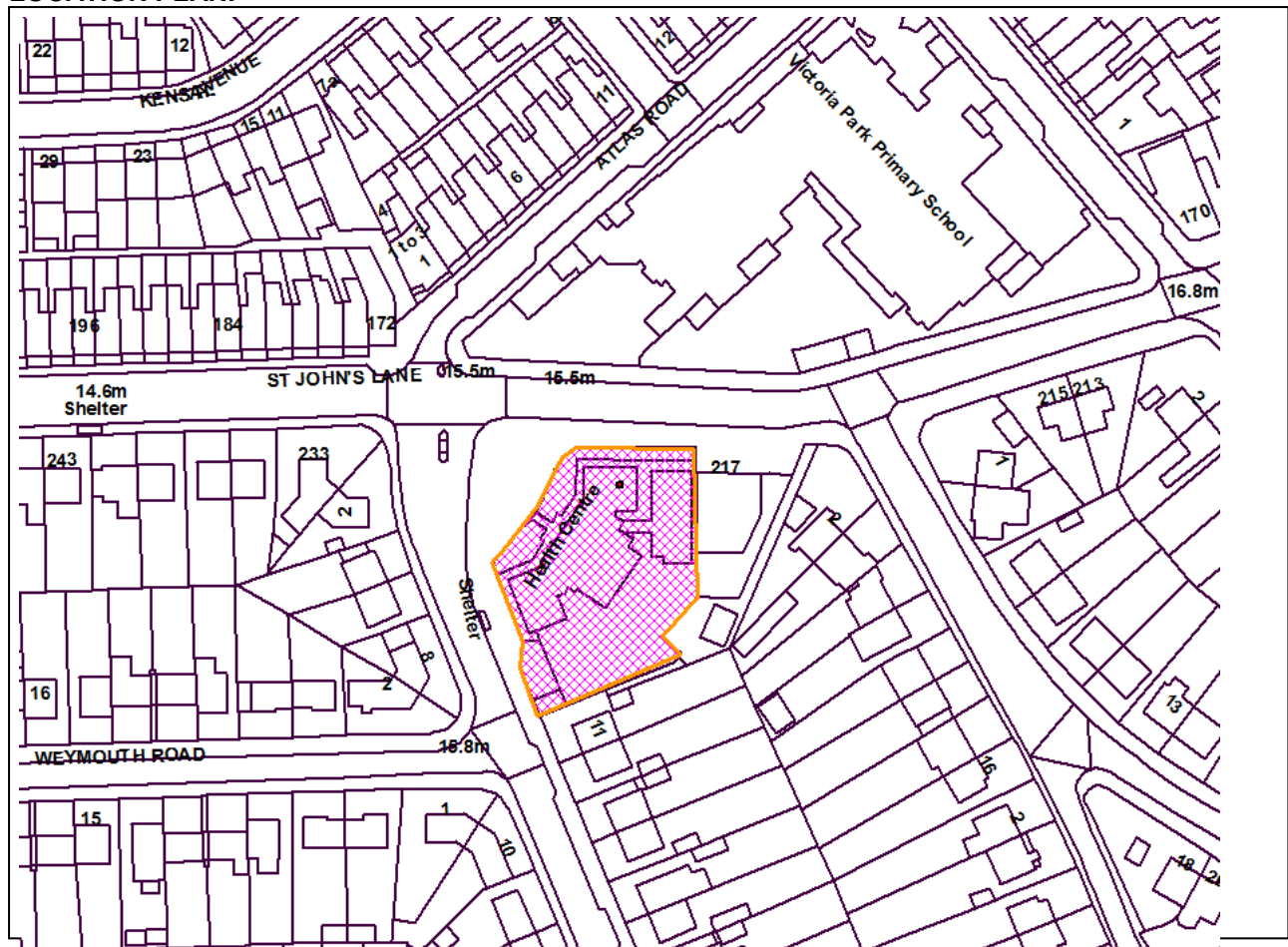
**RECOMMENDATION:** GRANT subject to Planning Agreement

**AGENT:** Cryer & Coe Architects  
10-12 Gloucester Road  
Third Floor  
Bristol  
BS7 8AE

**APPLICANT:** United Communities  
C/o Agent

*The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.*

### LOCATION PLAN:





**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS****SITE DESCRIPTION**

This application relates to the land at the corner of St John's Lane and Wedmore Vale, in Windmill Hill, south Bristol. The site is currently occupied by a pre-WWII, single storey building most recently used as a health centre and a vacant two-storey shop building, which was most recently used as a betting shop. The health centre building has not been in active use since 2018, when the Wedmore Practice relocated to the Marksbury Road Surgery. The betting shop has been vacant since 2019.

Access is provided to the site from Wedmore Vale and leads to a car park to the rear of the health centre with circa 12 car parking spaces. The shop includes an access from St. John's Lane.

The Malago Greenway cycle route runs along the edge of the site adjacent to Wedmore Vale and St John's Lane. The Victoria Park Primary School is located to the north of the site on the opposite side of St John's Lane and a vacant two-storey shop building (the William Hill building) is located to the east of the site. The remaining surrounding area is predominantly residential, made up largely of two-storey, semi-detached houses and some terraced houses.

The site is not located within a Conservation Area. There are no Listed buildings or structures within 200m of the site.

Consent was granted in January 2021 for the construction of 29no. flats on the site of the former health centre. This application follows a similar site and form, but expanded to include the shop on St John's Lane.

**APPLICATION**

The application seeks full planning permission for the demolition of the existing building on site and construction a part-four, part-three storey building containing 36 no. apartments (Use Class C3). The following housing mix is proposed:

- 16 no. one-bedroom, two-person apartments
- 12 no. two-bedroom, four-person apartments
- 2 no. three-bedroom, four-person apartments
- 3 no. three-bedroom, five-person apartments
- 3 no. three-bedroom, six-person apartments

The proposed flats would be for a mixture of social rent, affordable rent and shared ownership. 11 of the units would be secured via s.106 agreement as affordable housing for affordable rent in perpetuity.

The building would form an L-shape in plan, with a frontage along St John's Lane and a frontage along Wedmore Vale. The main pedestrian entrance to the building would be at the corner of Wedmore Vale and St John's Lane, with a large lobby and stairwell located within this area.

At ground floor level, the building would consist of four no. one-bedroom apartments, three no. two-bedroom apartments and two no. three-bedroom apartments. Each of the ground floor apartments would benefit from outdoor amenity space in the form of gardens along the frontages.

At first floor level, the building would consist of five no. one-bedroom apartments, four no. two-bedroom apartments and two no. three-bedroom apartments. The buildings would be accessed via a deck to the rear of the building. Each of the first floor apartments would benefit from a balcony.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

A further five one-bedroom apartments, four two-bedroom apartments and two no. three-bedroom apartments are proposed at second floor level. Each of the second floor apartments would benefit from a balcony and would be accessed via the rear deck.

The third floor would be recessed, located around away from the southern and eastern extremes of the roof and would contain two no. one-bedroom apartments, one no. two-bedroom apartment and two no. three-bedroom dwellings. This floor would be set back and not visible from the public realm. Each flat would benefit from a private terrace, located to the rear of the building.

The building would be constructed in red brick at ground, first and second floor level, with the fourth floor constructed in standing seam aluminium. The elevations facing the internal parking area would be rendered and the deck access would feature grey powder coated metal railings. The central lobby area would feature grey powder coated metal screens and all external windows and doors are proposed to be grey upvc.

14 car parking spaces would be provided to the rear of the building. Two of these spaces would include EV charging points. Two cycle stores are proposed with 59 spaces and a communal bin store would be included internally at the southernmost point of the building.

Air source heat pumps would provide heating and cooling to the building, and the roof level would feature solar pv panels.

**RELEVANT HISTORY**

19/05042/F - Redevelopment of existing site with demolition of existing NHS healthcare facility and creation of an affordable housing complex (29 Dwelling units). – GRANTED subject to condition(s) – January 2021.

**RESPONSE TO PUBLICITY AND CONSULTATION****Public Comments**

A total of six comments were submitted in response to the proposals as submitted from five interested parties.

Of the six comments on the application as submitted, five were in objection to the scheme.

The following issues were raised:

- Concerns about the level of parking proposed and the potential impact of the proposed development on local parking provision
- Concern about the impact of the proposed development on local highways and associated safety issues for pedestrians and vehicles
- Concern about the scale and massing of the proposed development relative to site context

One comment was neutral in its nature and raised a query in respect of the validity of the parking survey.

**INTERNAL CONSULTEES****City Design Group (CDG) – No objection**

In response to the scheme as originally submitted, the City Design Group officer provided the following comments:

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

“The proposed development builds on the existing approval at the health centre site, and offers an opportunity to repair the urban grain by comprehensively developing the frontage onto St. John’s Lane.

The height of the proposed building is the same as the existing consent and is considered acceptable. The proposed form and materials are generally acceptable and there is a good level of private amenity space proposed.

There are concerns about the relationship of the new, eastern end of the building with properties on St. John’s Crescent. Removal of the terrace at this end or inclusion of a screen to avoid overlooking is recommended. It is advised that the building is no taller than the existing William Hill building at this end of the site.

Rationalisation of the parking arrangement and access for pedestrians is recommended. It is expected that landscaping is provided to the rear of the building in some form to soften the boundary with adjoining occupiers and create a more attractive proposal.”

In response to the comments above, the applicant pulled the eastern elevation away from the boundary with St. John’s Crescent and details were provided of a screen to protect privacy.

The submission of a landscaping scheme will be conditioned as part of any approval onsite.

Transport Development Management (TDM) – No objection**Principle**

The application proposes to demolish the existing buildings and in their place construct a four storey block that will comprise 16 x one, 12 x two and eight x three bed flats, with associated car/cycle parking and waste storage. A previous application was granted permission earlier in the year, although this just comprised the former Health Centre site only – 19/05042/F. Following a number of revisions being made by the applicant, Transport Development Management (TDM) considers the proposals acceptable on highway safety grounds.

**Highway Network**

The site is located at the give way junction between Wedmore Vale and St John’s Lane (B3122), both of which are bus routes and subject to 20mph speed limits. Due to the number of vehicles using Wedmore Vale, it features both a left and right hand turn lane which are separated from vehicles turning off of St John’s Lane by a pedestrian island. This features tactile paving, illuminated keep left bollards and uncontrolled pedestrian crossing facilities with dropped kerbs incorporating tactile paving on both sides of the carriageway. Within the footways surrounding the site and running along both roads, is a segregated cycle lane which forms part of the Filwood Greenway.

Adjacent to the site is a bus stop with raised kerb, shelter and RTI display. Along Wedmore Vale in-between a series of kerb build outs are a number of on-street parking bays. On St Johns Lane there is a signalised pedestrian crossing. Both carriageways feature double yellow lines. There have been four recorded accidents at the junction. Two featured nose to tail collisions between two cars. The other two were due to turning movements from the major road, each between a car and cycle. All resulted in slight injuries with the exception of one of the cyclists, who sustained a serious injury.

**Transport Statement**

Due to the scale of the development a Transport Statement has been submitted. This sets out that the site is located within a predominately residential area and is served by four bus routes. Two of these are on Wedmore Vale (90 & 511) and the others are on St John’s Lane (91 & 512), all of which are

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

within a short walk. Bedminster Railway Station is within a moderate walk and the Filwood Greenway cycle route runs right around the sites boundary. As a result it concludes that the site is in a sustainable location, which TDM concurs with. The statement also includes an analysis of accident data within the proximity of the site. This concludes that all of the accidents can be contributed to driver error rather than any fundamental flaw with the design of the adopted highway. TDM agrees with this assessment. To determine the potential number of two-way trips the proposed usage would generate an assessment was made using data derived from TRICS.

This is an industry standard database of trip rates used to quantify the numbers of trips associated with new developments. This revealed that for the extant use over the course of a day 495 two-way car trips would be generated compared to just 60 for the proposed flats. Whilst this is a substantial reduction it should be noted that there would be more trips associated with the residential usage over the weekend than the former health centre. TDM considers the methodology used to undertake these calculations to be acceptable. Due to the sustainable location of the site the applicant proposes to provide a limited amount of off-street parking. To ascertain the amount of available on-street parking a parking survey using the council's methodology was undertaken on the 9th and 10th of September 2019 from 10pm until 12pm.

Within a distance of 150m this found that there were between 19 and 21 available spaces or 16 and 17 excluding areas where vehicles were parked on the footway. To determine the potential number of cars that could be generated by the site an analysis of car ownership data taken from the 2011 census for the 2011 super output area – lower layout E01014736 Bristol 040D was undertaken at TDM's request. This found that 31% of households did not own a car/van, 49% owned one car/van, 17% owned two cars/vans and 3% owned three or more car/vans. In the earlier application 19/05042/F the applicant proposed that as the development would consist purely of affordable housing it would be appropriate to apply a 31% ratio to non-car ownership and a 69% ratio to owning one car/van.

Whilst TDM understood how this calculation was reached it implied that someone living in affordable housing is less likely to own two cars. However, this ignored the fact that over time employment sites have become more spread out across the Greater Bristol area, incomes have increased and rents for affordable housing are higher than those of council housing provided by Bristol City Council. Whilst it is unlikely that the two/three bed households will own more than three vehicles, it is not considered unreasonable that some may own at least two. In these circumstances TDM considered it appropriate to apply a 31% ratio to non-car ownership, a 54% ratio to owning one car/van and a 15% ratio to owning two cars/vans.

The applicant has applied these ratios to this application therefore determining that the site would generate 29 vehicles. As there are thirteen off-street parking spaces this would leave 16 vehicles to be accommodated on-street which based on the parking survey results (excluding those parked on the footway) could be achieved without causing added parking stress. TDM concurs with this analysis.

### Travel Plan

A Travel Plan Statement has been provided. This will need to have an appointed Travel Plan Coordinator and associated budget for actions including bus vouchers and cycle vouchers. Previously a car club bay was proposed, although as this was unlikely to be commercially viable it has been removed and instead a contribution of £10,000 is proposed towards the cost and promotion of a car club bay that is within walking distance of the site. This is acceptable and would need to be collected through a Section 106 Agreement. A copy of the Travel Pack must be submitted for approval prior to occupation along with the Travel Plan Coordinators contact details.

### Section 106 Contributions

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

To promote public transport use and encourage a modal shift, a contribution for £10,000 towards a car club space is sought via Section 106.

**Footway**

To the front of the existing betting shop is a fully segregated cycle route with a footway to the rear which forms part of the Filwood Greenway. Currently the cycle route and footway have been designed as a vehicle crossover. As this will no longer be required both must be reinstated to full kerb height for which the applicant will be required to obtain an Excavation Licence online at [www.bristol.gov.uk/highwaylicences](http://www.bristol.gov.uk/highwaylicences) and enter into a Section 278 Agreement with the Council.

**Site Layout**

The applicant proposes to provide a rear car park that would be accessed via a private access road running along the southern boundary of the site. This will not feature a gate, which could result in unauthorised access and increase the risk of anti-social behaviour. A gate constructed to Secured By Design standards and electrically operated, set back at least 5m from the back edge of the footway is recommended. A hatched area should be provided to the rear, to indicate to motorists the extent to which the gate will open. To ensure that the vehicular visibility splay of the proposed access road will not be impinged the low level brick wall and box hedge has been pulled back slightly and the applicant has agreed to maintain it at no more than 0.6m high through a Management Strategy. This is acceptable.

Whilst a motorist would not be able to see any oncoming traffic should a bus be pulled up at the adjacent stop, given this arrangement already exists and there is no recorded accident history, it is unlikely that motorists would pull out until they believe it is safe to proceed, especially when taking into account the frequency of the services that use the stop and the time they would likely be stopped for.

The access road should be wide enough for two vehicles to pass, although in the event that they cannot, data has been submitted setting out that due to the limited number of parking bays the chances of this happening are extremely low. Suitable drainage must be provided at the point of access to prevent the discharge of any surface water onto the adopted highway as well as lighting to illuminate both the access road and the rear car park. Each of the parking spaces measures 2.4m x 4.8m with the disabled space having a 1.2m side and rear hatched area. Providing it is signed and marked accordingly this is acceptable. Swept path analysis has been submitted to demonstrate that vehicles can safely access bays 4, 7, 10, 11 & 13.

**Car Parking / Cycle Parking**

The applicant proposes to provide 13 parking bays of which one will be for disabled people and another will feature an Electric Vehicle Charging Point (EVCP). Based on the increased number of bays at least two EVCP's must be provided with the remaining having the necessary ducting, cabling and earthing to enable additional points to be installed in the future. Whilst a significant number of objections have been made by local residents, the results of the parking survey and analysis of car ownership levels indicates that providing Advice I045A) Restriction of Parking Permits – Future Controlled Parking Zone/Residents Parking Scheme is applied, any overspill parking can be safely accommodated within surrounding streets. In respect of cycle parking the applicant proposes to provide two stores that will be able to accommodate 80 cycles. This is well in excess of the minimum standard and is to be welcomed. The storage will consist entirely of Sheffield Stands. A further two Sheffield Stands will be provided at the main entrance to the building for the use of visitors which is acceptable. Motorbike parking will also be provided. This is acceptable.

**Waste**

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

Due to the length of time Bristol Waste vehicles would need to stop in order to empty the number of containers required, the applicant has relocated the bin store onto Wedmore Vale. Whilst this is not ideal, it is better than placing it on St John's Lane where the traffic flow is significantly heavier.

**Construction Management**

Due to the scale/location of the development and the impact on the adopted highway during the demolition/construction period the applicant must submit a Construction Management Plan.

A Highway Condition Survey will also be required. The extent of the area to be surveyed must be agreed with TDM. Both documents can be secured by condition.

Having addressed all issues TDM considers proposals acceptable on highway safety grounds.

**Affordable Housing – No objection**

Under Bristol Core Strategy (2011) policy BCS17 the site should be subject to 30% affordable housing requirement. This amounts to 10.5 units rounded to 11 and the proposed number to be secured via S106 is therefore policy compliant. As the scheme proposed is 100% affordable scheme it exceeds the policy requirement for affordable provision. It is agreed that 11 units should be secured in perpetuity as affordable housing for eligible households via a S106 agreement using the BCC standard clauses as set out in the Affordable Housing Practice Note (AHPN) 2018. The tenure of those units should be specified in the S106, with a caveat clause that the tenure/size mix can be altered with the written agreement of the Council in order to allow for any adjustments required in light of the new Homes England funding programme that will be coming into effect shortly.

**Tenure Mix**

The applicant has proposed that the 11 units (31%) named in the S106 as affordable in perpetuity should be specified as Affordable Rent tenure, with rents capped at 80% of market rent, or prevailing LHA levels, whichever is lower. However the Affordable Housing Practice Note (2018) would usually require the tenure/size breakdown for the S106 units as majority social rented (77%) and no more than 23% shared ownership where 30% Affordable Housing is required. In this instance it is noted that if the remaining non S106 affordable units are Affordable Rent this may create housing management issues as there will not be parity between rent levels in the same scheme. Therefore, in line with the Affordable Housing Practice Note 2018 policy on schemes where 20% affordable housing is required, and with the S106 agreement for the extant scheme on this site, it is accepted that all of the units secured via the S106 should be Affordable Rent in this instance.

The Councils Ward Profile for Windmill Hill (2020) indicates a larger than average number of younger households (25 – 39 years) and relatively high numbers of one and two bedroom households. There are a high proportion of households in private rented accommodation, suggesting that there may be demand for affordable intermediate units for young families/smaller households, see <https://www.bristol.gov.uk/documents/20182/436737/Windmill+Hill.pdf/9bbfbdbb-78f2-42c0-96bf-910a9316fcfc>

Therefore we will expect to see provision of a number of shared ownership units within the scheme overall, although these will not be secured by the S106 agreement. For affordability it would be preferable if the majority of shared ownership provision is one and two bed units.

Rents should be index linked to ensure affordability of housing units for prospective tenants and shared-ownership purchasers who cannot afford market sale or market rent. Service Charges should not exceed £250 per annum and £650 per annum in respect of an affordable flat index linked. Bristol City Council's policy towards rent levels in shared ownership as maximum 1.5% of retained equity in order to ensure affordability of intermediate units should be taken into account if at all possible.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**Contaminated Land Environmental Protection – No objection

The proposed development is sensitive to contamination and the submitted "desk study and site investigation report" prepared by T & P regeneration has identified some contaminants to be present above the screening criteria for the proposed end use.

Conditions are recommended to be applied to any future planning consent for the submission and implementation of a remediation scheme and the reporting of unexpected contamination.

Flood Risk Manager – No objection

We are satisfied that the proposed drainage arrangements are sufficient to mitigate the risk of surface water/sewer and groundwater flooding to the site and evidence that it will not cause flooding to surrounding sites.

This approval does not infer acceptance of all licences and permits, it is the applicant's responsibility to ensure all relevant permits and licences are obtained - specifically on applications which include outfalls.

Nature Conservation – No objection

The Council's Ecologist has raised no objection to the planning application, subject to planning conditions for the provision of bird and bat boxes, the clearance of vegetation and advisory notes for bats and hedgehogs.

Sustainable Cities – No objection

The energy statement confirms that MHVR will be used to mitigate any potential risk of overheating under present climatic conditions.

Good energy efficiency measures are proposed and the proposed use of communal and individual air source heat pumps in combination with PV would meet policy BCS14. Further detail of the pumps and the proposed PV should be secured via condition.

The Broadband Assessment shows that Fibre to the Premises is available and the provision of an EV charging point is noted. Further details of what is proposed should be secured via condition.

Environment Health – No objection

It is noted that there are a number of existing residential properties similar distances to both the school and St Johns Lane that whilst this road can be busy during the day it is less busy in the evening/night time. I also feel it would be likely that residents would expect some noise from the school and traffic at this location and it is unlikely to lead to any significant harm but I would just like to ensure that the property is suitably insulated, if necessary, against existing noise sources in the area.

I would therefore ask for a noise sensitive premises assessment to be submitted via condition should the application be approved

**RELEVANT POLICIES**

National Planning Policy Framework – February 2019

Bristol Local Plan, Comprising Core Strategy (Adopted June 2011), Site Allocation and Development Management Policies (Adopted July 2014) and The Bristol Central Area Plan (Adopted March 2015) (as appropriate) and SDP2 A guide for Designing House Alterations and Extensions (October 2005).

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

**KEY ISSUES****A. IS THE PROPOSED DEVELOPMENT ACCEPTABLE IN PRINCIPLE AND IS THE HOUSING TYPE AND MIX APPROPRIATE?**

Section 5 of the NPPF sets out the approach to 'Delivering a sufficient supply of homes'. It states the importance of having a sufficient amount and variety of land coming forward to meet housing requirements.

Policy BCS5 sets out that the Core Strategy aims to deliver new homes within Bristol's existing built up areas to contribute towards accommodating a growing number of people and households in the city. Between 2006 and 2026, 30,600 new homes will be provided in Bristol.

Policy BCS7 states that local shopping and service provision in smaller frontages or single shops away from the identified centres should be retained where it remains viable and provides an important service to the local community.

Policy BCS12 states that existing community facilities should be retained, unless it can be demonstrated that there is no longer a need to retain the use or where alternative provision is made.

Policy BCS18 supports a neighbourhood with a mix of housing tenures, types and sizes to meet the changing needs and aspirations of its residents.

Policy BCS20 states that development should maximise opportunities to re-use previously developed land.

Full planning permission is sought for the demolition of the former health centre building and adjacent betting shop. The health centre closed in 2018 and the site has been marketed by NHS Property Services for redevelopment. This site has been surplus to the requirements of in terms of a health / community use since the Marksbury Road surgery opened in 2018. The shop closed in 2019.

The proposed development would consist of the construction of a part four-; part three-storey building, with 16 no. one-bedroom apartments, 12 no. two-bedroom apartments and 7 no. three-bedroom apartments. Each of the apartments would fall under Bristol City Council's definition for affordable housing, and would be a mix of social rent, affordable rent and shared ownership.

Accordance with Policy BCS12 is demonstrated as the site is surplus to NHS requirements due to the nearby alternative provision and the fact that the site has been marketed and no offers for alternative community uses have been received.

Whilst the betting shop would have fallen into sui generis use, the unit has been considered as a potential retail unit (Use Class E) as the General Permitted Development Order allows for change of use to retail without requiring planning permission.

The unit is located away from a designated retail area, and has been vacant for two years. The length of vacancy is considered to demonstrate that the unit is not viable, and its use as a betting shop is not considered to constitute an important service to the local community. Alternative retail provision is available on St John's Lane and Wells Road.

The proposed development would contribute positively to housing supply within the Bristol and would be situated within the existing built up area as required by BCS5.



**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

It is considered that the need for affordable housing is considered to sufficiently outweigh the need to retain the previous community and retail uses on the site.

The site constitutes previously developed, brownfield land and the proposed development would represent the more efficient use of the site. This would accord with Policy BCS20.

The site is located on a high frequency bus route and within a sustainable location with sufficient access to amenities and services along St John's Lane. Further detail regarding this is provided within Key Issue D.

The principle of residential use on the site is considered acceptable and is supported by the NPPF and Policy BCS5.

Within the St John's Lane Local Super Output Area (LSOA), 72% of dwellings are houses; with the remaining 28% are flats, maisonettes or apartments. 25% of homes are social rented. 17% of dwellings in the LSOA have one 1 bedroom, 23% of dwellings have 2 bedrooms, 53% of dwellings have 3 bedrooms.

The proposed development would provide 16 no. one-bedroom apartments (equivalent to 44%), 12 no. two-bedroom apartments (33%), and 8 no. three-bedroom apartments (22%). It is considered that the proposed development would provide a diverse housing mix to cater to a variety of needs and would contribute to creating a mixed community.

It is considered that the proposed housing mix and type would be acceptable.

#### **B. WOULD THE PROPOSED DEVELOPMENT PROVIDE AN APPROPRIATE LEVEL OF AFFORDABLE HOUSING?**

Policy BCS17 of the Core Strategy sets out the requirement for affordable housing in the city. For the area in which the application site is located, the policy requires that any development of 15 dwellings or more should provide 30% of residential units as affordable housing.

All of the units would be affordable housing, with a mixture of social rent, shared ownership and affordable rent. 11 of these units would be secured as affordable housing in perpetuity via s.106 agreement. This would be equivalent to 31% and would therefore comply with Policy BCS17.

The proposed affordable housing units to be secured via s.106 agreement would be affordable rent. This is considered acceptable to the Affordable Housing team and would ensure consistency with the extant consent onsite.

It is considered that the proposed development would provide a sufficient level of affordable housing.

#### **C. WOULD THE PROPOSED DEVELOPMENT BE OUT OF SCALE OR CONTEXT WITH THE SURROUNDING AREA?**

Policy BCS20 sets out that an appropriate density should be informed by the characteristics of the site and the local context.

Policy BCS21 advocates that new development should deliver high quality urban design that contributes positively to an area's character and identity, whilst safeguarding the amenity of existing development.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

Policies DM26-29 (inclusive) of the Site Allocations & Development Management Policies require development to contribute to the character of an area through its layout, form, public realm and building design.

The existing site is occupied by two pre-WWII buildings of low architectural quality, with car parking to the rear. The William Hill building is of two-storeys with a pitched roof. Despite being the same number of storeys, this building is significantly taller than the adjoining semi-detached houses on St John's Crescent. The former health centre is of a single storey with a part-flat, part-pitched roof.

The surrounding area is largely residential and characterised by pre-WWII semi-detached houses and late 19<sup>th</sup> Century terraces. The site is not within a Conservation Area, and there are no Listed Buildings within a 200 metre radius.

The application seeks full planning permission for the construction of 36 apartments within a three-storey building, with a fourth recessed storey. The building would form an L-shape in plan, with two frontages; one along Wedmore Vale and one along St. John's Lane. A car park would be located behind the building.

City Design Group was consulted as part of the application process and noted that the current scheme represents an improvement on the previously approved scheme by providing a coherent frontage along St John's Lane.

The scale of three and four storeys matches that of the previously approved scheme. The fourth storey would be set back would not be visible from the street. Whilst generally four storeys is considered to be above the norm for this location, the recessed upper storey is considered to have limited impact upon the character of the surrounding area and is considered to be acceptable when applying an approach of urban intensification consistent with the Urban Living SPD.

The massing of the building at the eastern end matches that of the existing William Hill building and would be acceptable. The three storey section of the building fronting Wedmore Vale is considered acceptable as it manages the step up from two storeys at 11 Wedmore Vale and builds towards the corner on St Johns Lane.

The proposed L-shape of the building correctly addresses both frontages, whilst the variation in the elevation and elevational treatments effectively breaks down the elevation and creates amenity space in the form of the balconies.

The centrally located entrance and lobby creates a distinct architectural feature which is welcoming, attractive and easy to use in accordance with the Urban Living SPD.

The car parking has been effectively located to the rear of the building, limiting its impact upon the character of the area and the cycle storage is located in a safe, accessible location.

The building would be constructed in red brick along the frontages and side elevations, with a recessed standing seam top floor. The internal elevations facing the car park would be rendered.

The proposed use of red brick would accord with the existing character of the area, reflecting the character of the early 20<sup>th</sup> Century dwellings on Wedmore Vale and the Victoria Park School opposite. The use of standing seam and metal creates variation in the elevation and it is considered that the material palette balances modern and traditional aesthetics effectively.

A condition should be attached to any permission for further details of the materials proposed to ensure that the proposed development is constructed in a high quality manner that enhances the character of the site.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

It is considered that the proposed development would create a high-quality environment for future occupiers, in accordance with Policy BCS21. The use of deck accesses would enable all the units to be dual aspect, whilst defensible space is to be provided in front of bedroom and living room windows to reduce noise and disturbance. Each apartment would benefit from outdoor amenity space in the form of gardens or balconies. These balconies would create natural shading for the lower floors and mechanical ventilation would create ensure a cool environment in summer, and a warm one in winter.

A condition is to be attached to any permission for the provision of a landscaping scheme and boundary treatments within the gardens along the St Johns Lane and Wedmore Vale frontages.

It is considered that the proposed development would represent an enhancement in the appearance and character of the site, removing a low-quality building, and replacing with a strong architectural form. It is considered that the scale is appropriate when applying the principles of intensification set out in the Urban Living SPD, but also considers the context of the surrounding area.

#### **D. IS THE IMPACT OF THE PROPOSED DEVELOPMENT UPON TRANSPORT AND HIGHWAYS ACCEPTABLE?**

Policy BCS10 states that developments should be designed and located to ensure the provision of safe streets. Development should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

Policy DM23 of the Site Allocations and Development Management Policies outlines that development should not give rise to unacceptable traffic conditions and will be expected to provide safe and adequate access onto the highway network.

The proposed development would be accessed via the existing access from Wedmore Vale, adjacent to 11 Wedmore Vale. This shared access would lead to a car park with eight spaces, including one disabled space. Two electric vehicle charging points would also be provided.

The proposed development is considered to be in a sustainable location on a high frequency bus route and close to shops and services.

The applicant has provided trip rates for the existing and proposed development. These industry standard rates, known as 'TRICS', outline that there would be a significant reduction in the number of two-way car trips as a result of former health centre use. It is estimated that the community use would have created over 400 trips per day, with the proposed residential use estimated to be around 60 trips. As such, it is expected that there would be a benefit in terms of traffic impacts for residential compared to the previous use, or a potential reuse as a community facility.

The TDM response details that a total of 29 vehicles would be generated by the site, for which there would be 13 off-street car parking spaces provided. A car parking survey has been undertaken to ascertain whether the other estimated 16 vehicles can be accommodated on-street. Across the surveys, a total of 16 and 17 spaces were identified and TDM conclude that there would be sufficient on-street parking to absorb any overspill parking safely.

A Travel Plan is to be implemented to encourage modal shift to public transport and active travel and a contribution would be made to the Car Club to reduce demand for private vehicle car parking / trips.

Vehicle tracking has been used to determine that all the car parking spaces can be utilised, and the cycle parking is considered to be acceptable both in terms of the number of stands, the location and accessibility. The vehicle charging points are provided in accordance with the Parking Standards Schedule.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

The proposed bin storage is sufficiently large enough to accommodate the required refuse and recycling provision and the presentation point located to the front of the building would be accessible for Bristol Waste. A condition should be attached to any permission to ensure waste is not left to the front of the building and stored appropriately.

A construction management plan and highway condition survey would sufficiently address and manage any construction impacts. These would both be secured via condition.

The proposed development would sufficiently address transport and highways impacts.

**E. WOULD THE PROPOSED DEVELOPMENT BE ACCEPTABLE ON RESIDENTIAL AMENITY?**

Policy BCS21 outlines that development in Bristol is expected to safeguard the amenity of existing development and create a high-quality environment for future occupiers.

Policy DM29 sets out that new buildings will be expected to ensure that existing and proposed development achieves appropriate levels of privacy, outlook and daylight.

BRE Fact Sheet 1 sets out that if new development falls beneath a line drawn at 25° from the horizontal, then there is unlikely to be a substantial effect on daylight and sunlight.

The applicant has provided a shadow study (see Design and Access Statement, section 4.5). This demonstrates that due to the orientation of the development to the north of residential properties there would be virtually no shadowing of neighbouring homes on Wedmore Vale. The proposed development would not result in any further overshadowing of homes on St John's Crescent due to the proposed development being of a similar scale and form to the William Hill building. The proposed building would be situated sufficiently far from Victoria Park School so as to not cause unacceptable harm to the amenity of pupils and staff through overshadowing.

There would be a slight increase in overlooking of 6 and 8 Wedmore Vale; however the proposed building would be situated in excess of the 21 metre separation distance (22 metres). Overlooking has been managed throughout with the varied nature of the elevation, whereby each window would be angled to not directly face neighbouring homes. The proposed development would result in a slight increase in overlooking of the school playground, but views would be screened almost entirely by the mature trees that line St John's Lane. Homes to the rear of the site on St John's Crescent and the eastern side of Wedmore Vale would be situated comfortably beyond the 21 metre separation distance from the proposed development so as to not cause overlooking.

The proposed development does include deck access and roof terraces which could increase overlooking. Whilst the proposed balconies on the St. John's Lane and Wedmore Vale elevation would be situated sufficiently far from existing homes to avoid unacceptable overlooking, a privacy screen will be provided on the roof terraces to avoid overlooking towards 11 Wedmore Vale and no's 2 and 4 St John's Crescent.

It is considered the proposed terraces and balconies would be located sufficiently far from other residences to avoid causing unacceptable harm to residential amenity through noise and disturbance.

The proposed development would be situated sufficiently far from neighbouring properties so as to not be overbearing in its nature.

The proposed dwellings are considered acceptable in terms of providing sufficient space and flexibility for future occupiers would meet the nationally described space standards. Further detail on how the development creates a positive environment for future residents is set out in Key Issue C.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

It is concluded that the proposed development would not result in any unacceptable impacts on residential amenity.

**F. DOES THE PROPOSED DEVELOPMENT GIVE SUFFICIENT CONSIDERATION OF SUSTAINABLE DESIGN AND CONSTRUCTION?**

Policy BCS13 sets out that development should contribute to both mitigating and adapting to climate change, and to meeting targets to reduce carbon dioxide emissions.

Policy BCS14 sets out that development in Bristol should include measures to reduce carbon dioxide emissions from energy use by minimising energy requirements, incorporating renewable energy sources and low-energy carbon sources. Development will be expected to provide sufficient renewable energy generation to reduce carbon dioxide emissions from residual energy use in the buildings by at least 20%.

Policy BCS15 sets out that sustainable design and construction should be integral to new development in Bristol. Consideration of energy efficiency, recycling, flood adaption, material consumption and biodiversity should be included as part of a sustainability or energy statement.

The site is within a sustainable location, adjacent to public transport links and within walking distance of shops and services on St John's Lane.

The proposed development includes good energy efficiency measures to reduce energy demand and renewable energy in the form of roof mounted PV and air source heat pumps to further reduce CO2 emissions. The Energy Statement submitted with the application sets out that the proposed new dwellings would reduce CO2 emissions by approximately 20%. This meets the requirements of Policy BCS14. A plan should be provided via condition demonstrating the location of and size of the proposed solar PV panels and the air source heat pumps.

The proposed development would include mechanical ventilation heat recovery to limit risk of overheating.

The proposed development would remove two small trees on site, one of which has died and replace them with six new trees. Bird and bat boxes have been requested by the Nature Conservation officer. The details of these are to be secured via condition.

It is considered that the proposed development has given sufficient consideration of sustainability and would accord with Policies BCS13, BCS14 and BCS15.

**CONCLUSION AND PLANNING AGREEMENT**

The proposed development would contribute to the delivery of new, affordable homes on previously developed land, and is of an appropriate type and mix for the area. The design of the proposed dwellings is considered acceptable and there would be no unacceptable impacts upon residential amenity. Subject to conditions, the proposed development would be acceptable in terms of transport and highways. The proposed development would sufficiently reduce CO2 emissions and considers sustainable design and construction.

It is recommended that planning permission is granted subject to conditions, and the agreement of the following planning obligations:

- Provision of 11 affordable housing units (equivalent to 30% of the total), to be secured in perpetuity.
- Contribution to travel plan audit and management / travel plan implementation:
- Contribution to car club: £10,000.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS****RECOMMENDED GRANT subject to planning agreement****Time limits for commencement of development****1. Full Planning Permission**

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

**Pre commencement conditions****2. Approval of road works necessary**

No development shall take place until general arrangement plan(s) to a scale of 1:200 showing the following works to the adopted highway has been submitted to and approved in writing by the Local Planning Authority:

- Reinstatement and reconstruct footway/cycle lane to full kerb height using matching materials in place of redundant vehicle crossover on St Johns Lane

Where applicable indicating proposals for:

- Existing levels of the finished highway tying into building threshold levels
- Alterations to waiting restrictions or other Traffic Regulation Orders to enable the works
- Signing, street furniture, street trees and pits
- Structures on or adjacent to the highway
- Extent of any stopping up, diversion or dedication of new highway (including all public rights of way shown on the definitive map and statement)

No development shall take place over the route of any public right of way prior to the confirmation of a Town & Country Planning Act 1990 path diversion/stopping up order.

Prior to occupation these works shall be completed to the satisfaction of the Highway Authority and approved in writing by the Local Planning Authority.

**3. Construction Management Plan**

No development shall take place, including any demolition works, until a construction management plan or construction method statement has been submitted to and approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the demolition/construction period. The plan/statement shall provide for:

- 24 hour emergency contact number;
- Hours of operation;
- Parking of vehicle of site operatives and visitors (including measures taken to ensure satisfactory access and movement for existing occupiers of neighbouring properties during construction);
- Routes for construction traffic;
- Locations for loading/unloading and storage of plant, waste and construction materials;
- Method of preventing mud being carried onto the highway;
- Measures to protect vulnerable road users (cyclists and pedestrians)
- Any necessary temporary traffic management measures;
- Arrangements for turning vehicles;
- Arrangements to receive abnormal loads or unusually large vehicles;

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

- Methods of communicating the Construction Management Plan to staff, visitors and neighbouring residents and businesses.

Reason: In the interests of safe operation of the adopted highway in the lead into development both during the demolition and construction phase of the development.

**4. Highway Condition Survey**

No development shall take place (including investigation work, demolition, siting of site compound/welfare facilities) until a survey of the condition of the adopted highway has been submitted to and approved in writing by the Local Planning Authority. The extent of the area to be surveyed must be agreed by the Highways Authority prior to the survey being undertaken. The survey must consist of:

- A plan to a scale of 1:1000 showing the location of all defects identified;
- A written and photographic record of all defects with corresponding location references accompanied by a description of the extent of the assessed area and a record of the date, time and weather conditions at the time of the survey.

No building or use hereby permitted shall be occupied or the use commenced until any damage to the adopted highway has been made good to the satisfaction of the Highway Authority.

Reason: To ensure that any damage to the adopted highway sustained throughout the development process can be identified and subsequently remedied at the expense of the developer.

**5. Submission of Remediation Scheme**

Following demolition no construction shall take place until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be submitted to and approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

**6. Submission of samples before specified elements started**

Samples of the proposed brickwork, sheet metal, render, metal work and proposed materials for decking, windows and doors shall be submitted to and be approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved samples before the building is occupied.

Reason: In order that the external appearance of the building is satisfactory.

**7. Submission and Approval of Landscaping Scheme**

No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a scheme of hard and soft landscaping. The scheme will include details of tree locations, species, a maintenance schedule for watering and aftercare to ensure establishment, tree pits proposed for the trees.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

The approved scheme shall be implemented so that planting can be carried out no later than the first planting season following the occupation of the building or the completion of the development whichever is sooner.

All planted materials shall be maintained for five years and any trees removed, during, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the Council gives written consent to any variation.

Reason: To protect or enhance the character of the site and the area and to ensure its appearance is satisfactory.

#### 8. Renewable energy – further detail

Prior to implementation, details of the PV panels and Air Source Heat Pumps (including the exact locations, dimensions, design, technical specification) together with calculation of energy generation and associated CO<sub>2</sub> emissions to achieve 20% reduction on residual emissions from renewable energy in line with the approved energy statement should be submitted to the Local Planning Authority and approved in writing. The renewable energy technology shall be installed prior to occupation of the dwellings and thereafter retained.

Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions

#### 9. Noise Sensitive Premises Assessment

No development shall take place until a detailed acoustic report on the existing noise climate at the development site has been submitted to and been approved in writing by the Local Planning Authority.

The report shall include a scheme of noise insulation measures for all residential accommodation. The noise insulation measures shall be designed to achieve noise insulation to a standard that nuisance will not be caused to the occupiers of residential accommodation.

The noise assessment shall be carried out by a suitably qualified acoustic consultant/engineer and shall take into account the provisions of BS 8233: 2014 Guidance on sound insulation and noise reduction for buildings.

The approved scheme shall be implemented prior to the commencement of the use and be permanently maintained thereafter.

Reason: In order to safeguard the amenities of nearby occupiers. The details are needed prior to the start of work as the acoustic report may require changes to the design details.

#### 10. Nesting birds

No clearance of vegetation or structures suitable for nesting birds, shall take place between 1<sup>st</sup> March and 30<sup>th</sup> September inclusive in any year without the prior written approval of the local planning authority. The authority will require evidence provided by a suitably qualified ecological consultant that no breeding birds would be adversely affected before giving any approval under this condition. Where checks for nesting birds are required they shall be undertaken by a qualified ecological consultant no more than 48 hours prior to the removal of vegetation or the demolition of, or works to buildings.

Reason: To ensure that wild birds, building or using their nests are protected. All species of wild birds, their eggs, nests and chicks are legally protected until the young have fledged.



**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS****Pre occupation****11. Further details of proposed screens**

Prior to the occupation of the development, the screens detailed in Plan ref. SK007 'Typical Privacy Screen Details' shall be installed in accordance with the approved plans and retained in perpetuity.

Reason: To protect the amenity of adjoining occupiers.

**12. Implementation of approved remediation scheme**

In the event that contamination is found, no occupation of the development shall take place until the approved remediation scheme has been carried out in accordance with its terms. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report (otherwise known as a validation report) that demonstrates the effectiveness of the remediation carried out must be produced, and be approved in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors

**13. Reporting of Unexpected Contamination**

In the event that contamination is found at any time that had not previously been identified when carrying out the approved development, it must be reported immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the Environment Agency's 'Land Contamination: risk management' guidance and BS 10175:2011 + A2:2017: Investigation of Potentially Contaminated Sites - Code of Practice. Where remediation is necessary a remediation scheme must be prepared which ensures the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors. This is in line with paragraph 170 of the National Planning Policy Framework.

**14. Energy and Sustainability in accordance with statement**

The development hereby approved shall incorporate the energy efficiency measures, renewable energy, sustainable design principles and climate change adaptation measures into the design and construction of the development in full accordance with the energy and sustainability statement prior to occupation.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

Reason: To ensure the development incorporates measures to minimise the effects of, and can adapt to a changing climate in accordance with policies BCS13 (Climate Change), BCS14 (Sustainable energy), BCS15 (Sustainable design and construction) and DM29 (Design of new buildings).

**15. Implementation / Installation of Refuse Storage and Recycling Facilities – Shown on approved plans**

No building or use hereby permitted shall be occupied or the use commenced until the refuse store, and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the highway or pavement, except on the day collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises, protect the general environment, and prevent obstruction of pedestrian movement, and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

**16. Completion of Pedestrians/Cyclists Access – Shown on approved plans**

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

**17. Completion and Maintenance of Car/Vehicle Parking – Shown on approved plans**

No building or use hereby permitted shall be occupied or the use commenced until the car/vehicle parking area shown on the approved plans has been completed, and thereafter, the area shall be kept free of obstruction and available for the parking of vehicles associated with the development.

Reason: To ensure that there are adequate parking facilities to serve the development.

**18. Completion and Maintenance of Cycle Provision – Shown on approved plans**

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

**19. Management and Maintenance of Private Streets**

No building or use hereby permitted shall be occupied or use commenced until details of arrangements for the future management and maintenance of proposed carriageways, footways, footpaths and landscaped areas not put forward for adoption within the site has been submitted to and approved in writing by the Local Planning Authority. Following occupation of the first dwelling on the site, the streets shall be maintained in accordance with the approved management and maintenance details.

Reason: To ensure that all private streets and landscaped areas are appropriately managed and maintained to ensure the safety of all users.

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS****20. Provision of Vehicular Visibility Splays**

No building or use hereby permitted shall be occupied or use commenced until visibility splays 2.4 metres back from the centre line of the access and extending 25m metres on the nearside carriageway edge shall be provided at all accesses/junctions, as shown on the approved plans. Nothing shall be erected, retained, planted and/or allowed to grow at or above a height of 0.6 metres above the nearside carriageway level which would obstruct the visibility splay. The visibility splays shall be maintained free of obstruction at all times thereafter for the lifetime of the development.

Reason: To ensure motorists have clear and unrestricted views of approaching cyclists/vehicles in the interest of highway safety.

**21. Electric Vehicle Charging Points**

No building or use hereby permitted shall be commenced until details of Electrical Vehicle Charging infrastructure, management plan and phasing for implementation has been submitted to and approved in writing by the Local Planning Authority. This shall include details of the following:

- Final Layout
- Number and location of EV parking spaces
- Number and location of EV charging points
- Type of EV charging points (fast, rapid)
- Indicative locations for feeder pillars and protective infrastructure
- Evidence of power supply from WPD (to ensure substation capacity is adequate)
- Indicative location of substation (where required)
- Indicative cable routing
- Management plan outlining proposed management of spaces, charging network and infrastructure
- Electrical Layout and Schematic Design
- Feeder Pillar Design/Electrical Layout/Schematic Layout Designs

The Electric Vehicle Charging Points and management strategy as approved shall be implemented prior to occupation / as per the agreed phasing plan and retained in that form thereafter for the lifetime of the development.

Reason: To promote sustainable travel, aid in the reduction of air pollution levels and help mitigate climate change.

**22. Car Park Management Plan**

No building or use hereby permitted shall be occupied or use commenced until a car park management plan setting out how the car park will be managed has been prepared, submitted to and approved in writing by the Local Planning Authority. The measures shall thereafter be implemented in accordance with the approved car park management plan for the lifetime of the development.

Reason: To ensure the safe operation of approved car park(s).

**23. Bird and bat boxes**

Prior to occupation of the development details provided by a qualified ecological consultant shall be submitted to and approved in writing by the Local Planning Authority providing the specification, orientation, height and location for built-in bird nesting and bat roosting opportunities integrated within buildings and shown on a site plan with compass directions marked on it. This shall include six built-in swift bricks or boxes and two built-in bat boxes.

Bird boxes shall be installed to face between north and east to avoid direct sunlight and heavy rain. Swift boxes or bricks shall be provided in pairs or groups (e.g. at least two or three on a building,

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

avoiding windows) on north, north-east or east facing walls, at least 5 metres high, so that there is a clear distance (drop) below the swift boxes/bricks of 5 metres or more. Swift boxes shall be located under eaves where present.

Bat boxes shall face south, between south-east and south-west. Bat boxes shall be erected at a height of at least four metres, close to hedges, shrubs or tree-lines and avoid well-lit locations. Bat boxes which are being placed on buildings shall be placed as close to the eaves (if present) as possible. Development shall be undertaken in accordance with the approved details.

Reason: To help conserve legally protected bats and birds which include priority species.

**Post occupation management****24. Retention of Garage/Car Parking Space(s)**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) the garage/car parking space(s) hereby permitted shall be retained as such and shall not be used for any purpose other than the garaging of private motor vehicles associated with the residential occupation of the property and ancillary domestic storage without the grant of further specific planning permission from the Local Planning Authority.

Reason: To retain garage/car space for parking purposes.

**25. Travel Plan Statement – Submitted**

The Travel Plan Statement hereby approved shall be implemented in accordance with the measures set out therein.

Within three months of occupation, evidence of the implementation of the measures set out in Travel Plan Statement shall be prepared, submitted to and agreed in writing with the Local Planning Authority unless alternative timescales are agreed in writing.

Reason: To support sustainable transport objectives including a reduction in single occupancy car journeys and the increased use of public transport, walking and cycling.

**List of Approved Plans and Drawings****25. List of Approved Plans and Drawings**

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

0500-REV 1.1, Revised Site Location Plan received 21 Apr 2021  
 0501-REV 1.1, Revised Existing Site Plan received 21 Apr 2021  
 0502-REV 1.0, Revised Existing Elevations received 21 Apr 2021  
 1100-REV 1.5, Revised Ground Floor Plan received 21 Apr 2021  
 1101-REV 1.2, Revised First Floor Plan received 21 Apr 2021  
 1102-REV 1.2, Revised Second Floor Plan received 21 Apr 2021  
 1103-REV 1.3, Revised Third Floor Plan received 21 Apr 2021  
 1300-REV 1.1, Revised Roof Plan received 21 Apr 2021  
 1400-REV 1.1, Revised Ga Elevations received 21 Apr 2021  
 1401-REV 1.1, Revised Ga Elevations received 21 Apr 2021  
 1600-REV 1.1, Revised Site Sections received 21 Apr 2021  
 2104-REV 1.1, Revised Apartment Plans received 21 Apr 2021

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS**

2105-REV 1.1, Revised Apartment Plans received 21 Apr 2021  
 SK007, Typical Privacy Screen Details received 21 Apr 2021  
 SK010, Boundary Fence Details received 21 Apr 2021

Energy and Sustainability Strategy, received 8 October 2020  
 Design and Access Statement, received 8 October 2020  
 Desk Study and Ground Investigation, received 8 October 2020  
 Surface Water Storage Details, received 8 October 2020  
 Surface Water and Drainage Strategy, received 8 October 2020  
 Travel Plan Statement, received 8 October 2020

Reason: For the avoidance of doubt.

**Advices****1. Impact on the highway network during construction**

The development hereby approved and any associated highway works required, is likely to impact on the operation of the highway network during its construction (and any demolition required). You are advised to contact the Highway Authorities Network Management Team at [traffic@bristol.gov.uk](mailto:traffic@bristol.gov.uk) before undertaking any work, to discuss any temporary traffic management measures required, such as footway, Public Right of Way, carriageway closures or temporary parking restrictions a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary Traffic Management measures to be agreed.

**2. Restriction of parking permits – future controlled parking zone/residents parking scheme**

You are advised that the Local Planning Authority has recommended to the Highways Authority that on the creation of any Controlled Parking Zone/Residents Parking Scheme area which includes the development, that the development shall be treated as car free / low-car and the occupiers are ineligible for resident parking permits as well as visitors parking permits if in a Residents Parking Scheme.

**3. Highway Condition Survey**

The development hereby approved includes the carrying out of a Highway Condition Survey. To agree the extent of the area to be surveyed contact the Highway Authority's Transport Development Management Team at [transportDM@bristol.gov.uk](mailto:transportDM@bristol.gov.uk)

**4. Excavation Works on the Adopted Highway**

The development hereby approved includes the carrying out of excavation works on the adopted highway. You are advised that before undertaking any work on the adopted highway you will require a Section 171 (Excavation) Licence from the Highway Authority which is available at [www.bristol.gov.uk/highwaylicences](http://www.bristol.gov.uk/highwaylicences)

**5. Private Road**

You are advised that as a result of the proposed layout and construction of the internal access road, the internal access road will not be accepted for adoption by the Highway Authority under Section 38 of the Highways Act 1980. The development will be bound by Sections 219 to 225 (the Advance Payments Code) of the Highways Act 1980, unless and until you agree to exempt the access road. The exemption from adoption will be held as a Land Charge against all properties within the application boundary. Contact the Highway Authorities Transport Development Management Team at [DMengineering@bristol.gov.uk](mailto:DMengineering@bristol.gov.uk)

**Development Control Committee B – 9 June 2021****Application No. 20/04678/F : St Johns Lane Health Centre St Johns Lane Bristol BS3 5AS****6. Street Name and Numbering**

You are advised that to ensure that all new properties and streets are registered with the emergency services, Land Registry, National Street Gazetteer and National Land and Property Gazetteer to enable them to be serviced and allow the occupants access to amenities including but not limited to; listing on the Electoral Register, delivery services, and a registered address on utility companies databases, details of the name and numbering of any new house(s) and/or flats/flat conversion(s) on existing and/or newly constructed streets must be submitted to the Highway Authority.

Any new street(s) and property naming/numbering must be agreed in accordance with the Councils Street Naming and Property Numbering Policy and all address allocations can only be issued under the Town Improvement Clauses Act 1847 (Section 64 & 65) and the Public Health Act 1925 (Section 17, 18 & 19). Please see [www.bristol.gov.uk/registeraddress](http://www.bristol.gov.uk/registeraddress)

**7. Travel Plan Statement / Travel Plan – Not Submitted**

You are advised that a Travel Plan Statement / Travel Plan is required to be prepared and submitted using the Travel Plan Guide for New Developments and the associated templates at [www.bristol.gov.uk/travelplans](http://www.bristol.gov.uk/travelplans)

**8. Bats**

All species of bats and their roosts are legally protected. If bats are encountered all demolition or construction work should cease and an ecological consultant or the Bat Conservation Trust (Tel 0845 1300 228) should be consulted for advice.

**9. Hedgehogs**

Any undisturbed areas including where concentrations of leaves have collected should be checked for the presence of hedgehogs by an ecological consultant or contractor prior to demolition of the buildings. If hedgehogs are encountered they should be picked up wearing gloves, placed in a container and released outside the development footprint area in suitable habitat such as scrub, hedgerow or woodland.

## **Supporting Documents**

### **2. St Johns Lane Health Centre**

1. Site Location Plan
2. Ground Floor Plan
3. First Floor Plan
4. Second Floor Plan
5. Third Floor Plan
6. Proposed Elevations
7. Proposed Elevations





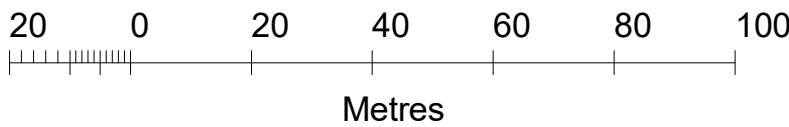
© Crown copyright and database rights 2019 OS 100035409 Reproduction in whole or in part is prohibited without the permission of Ordnance Survey.

1

0500

SITE LOCATION PLAN

SCALE 1:1250



NORTH

0m 10m 25m 50m 100m

Scale 1:1250

It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement

- Notes :
1. Drawings to be read in conjunction with all other disciplines drawings and specification documents
  2. Drawings are shown for design intent only and subject to further development.
  3. All dimensions are in millimetres unless otherwise specified.
  4. The level of information in this drawing is issued for information only. The information shown should not be used for any other purpose (such as building regulations, costing or construction) unless permission is expressly given by client/architect. To be read in conjunction with Civil, Structural and MEP information. Rights for party wall and rights of light lies with the client. A specialist measured survey has not been carried out on the site. All dimensions should be checked out on site. Illustrated material subject to copyright.

**SITE PERTAINING TO APPLICATION**

**FORMER NHS HEALTH CENTRE &  
WILLIAM HILL SITE  
ST JOHN'S LANE  
BEDMINSTER  
BRISTOL  
BS3 5AS**

1.1	09-04-2021	RED LINE BOUNDARY AMENDMENT	SC	DC	MC
1.0	30-09-2020	PLANNING	SC	DC	MC
Rev.	Date	Description	By	Chk'd	App'd

Drawing Status **PLANNING** Substantity **S0**

**Cryer&Coe architects**

10 - 12 Gloucester Rd,  
Third Floor,  
Bristol,  
BS7 8AE

Tel: 01173 634033  
hello@cryerandcoe.co.uk  
www.cryerandcoe.co.uk



Project Title

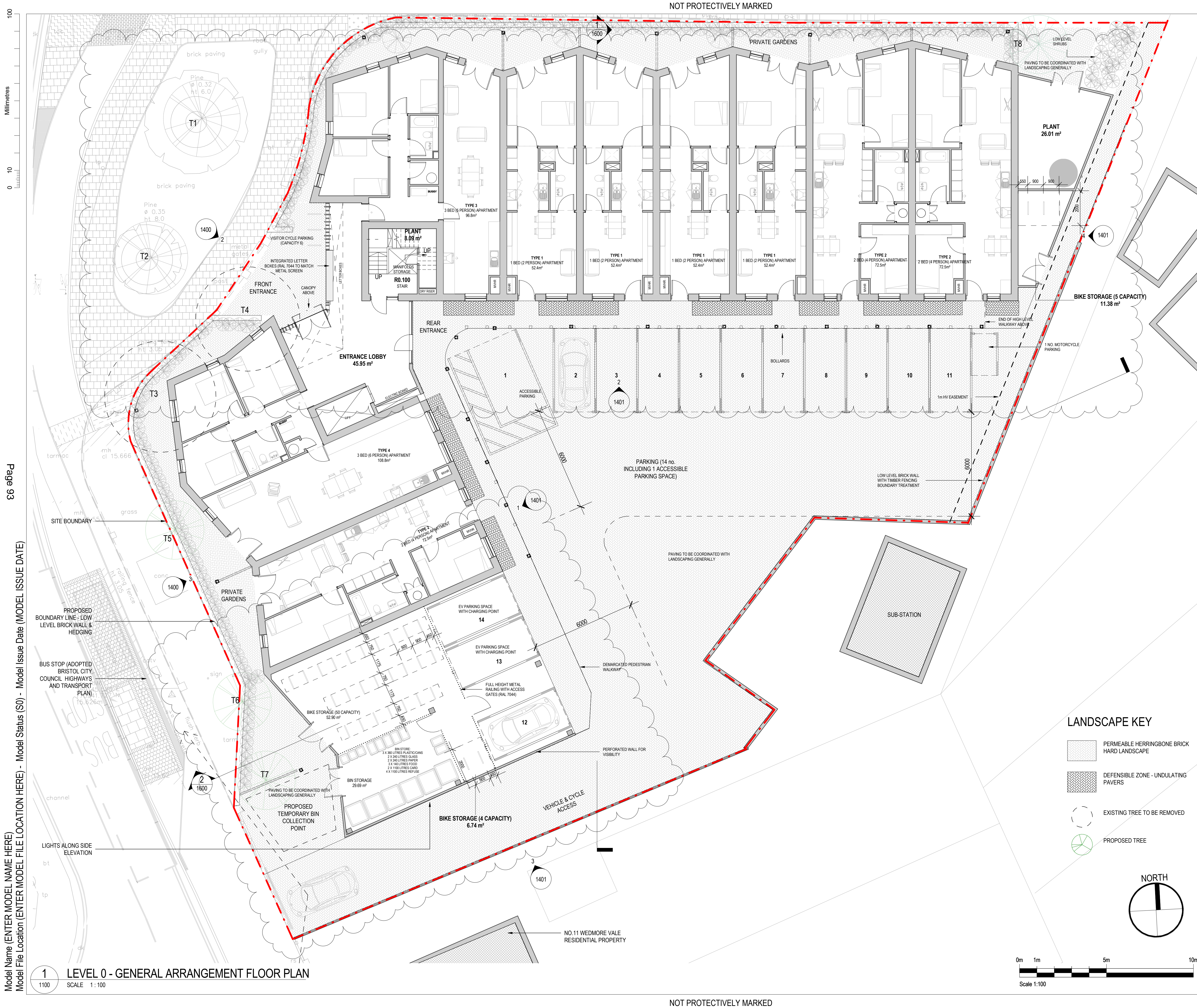
**ST JOHNS LANE  
BEDMINSTER, BRISTOL, BS3 5AS**

Drawing Title

**SITE LOCATION PLAN**

Scale	Designed	Drawn	Checked	Authorised
1 : 1250	SC	SC	DC	MC
Original Size	Date	Date	Date	Date
A1	30-09-2020	30-09-2020	30-09-2020	30-09-2020
Drawing Number	Revision			
SJL-CC-00-PL-A-0500	1.1			





It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement

- Notes:
1. Drawings to be read in conjunction with all other disciplines drawings and specification documents
  2. Drawings are shown for design intent only and subject to further development.
  3. All dimensions are in millimetres unless otherwise specified.
  4. The level of information in this drawing is issued for information only. The information shown should not be used for any other purpose (such as building regulations, costing or construction) unless permission is expressly given by client/architect. To be read in conjunction with Civil, Structural and MEP information. Rights for party wall and rights of light lies with the client. A specialist measured survey has not been carried out on the site. All dimensions should be checked out on site. Illustrated material subject to copyright.

Master Apartment Schedule				
Level	Type	Family	Count	
Level 0	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	4	
Level 0	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	3	
Level 0	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 0	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 0: 9				
Level 1	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	5	
Level 1	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	4	
Level 1	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 1	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 1: 11				
Level 2	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	5	
Level 2	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	4	
Level 2	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 2	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 2: 11				
Level 3	TYPE 5	1 BED (2 PERSON) APARTMENT - 52.5m² GIA	1	
Level 3	TYPE 6	1 BED (2 PERSON) APARTMENT - 52.5m² GIA	1	
Level 3	TYPE 7	3 BED (4 PERSON) APARTMENT - 79.5m² GIA	1	
Level 3	TYPE 8	2 BED (4 PERSON) APARTMENT - 74.3m² GIA	1	
Level 3	TYPE 9	3 BED (4 PERSON) APARTMENT - 85.5m² GIA	1	
Level 3: 5				
Grand total: 36				

Level 0 - Apartment Schedule				
Level	Type	Family	Count	
Level 0	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	4	
Level 0	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	3	
Level 0	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 0	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 0: 9				
Grand total: 9				

1.5	09-04-2021	RED LINE BOUNDARY, BUILDING FOOTPRINT, BIN, BIKE AND TRANSPORT AMENDMENTS	SC	DC	MC
1.4	09-02-2021	WIP CLIENT REVIEW	SC		
1.3	04-02-21	WIP CLIENT REVIEW	SC		
1.2	29-01-2021	WIP CLIENT REVIEW	SC	DC	MC
1.1	18-12-2020	WIP LAYOUT UPDATE	SC	DC	MC
1.0	30-09-2020	PLANNING	SC	DC	MC
Rev.	Date	Description	By	Chk'd	App'd

Drawing Status **PLANNING** Substantivity **SO**

**Cryer & Coe architects**  
10 - 12 Gloucester Rd,  
Third Floor,  
Bristol,  
BS7 8AE

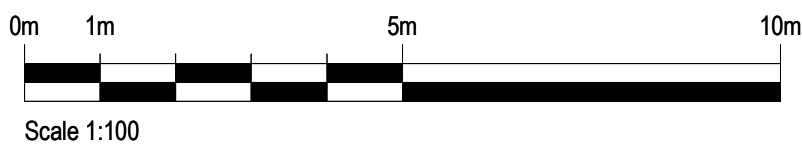
Tel: 01173 634033  
hello@cryerandcoe.co.uk  
www.cryerandcoe.co.uk



more than just a roof

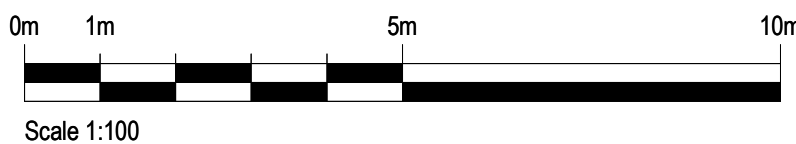
Project Title <b>ST JOHNS LANE BEDMINSTER, BRISTOL, BS3 5AS</b>					
Drawing Title <b>LEVEL 0 - GENERAL ARRANGEMENT PLAN PROPOSED</b>					
Scale 1 : 100	Designed SC	Drawn SC	Checked DC	Authorised MC	
Original Size A1	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020	
Drawing Number <b>SJL-CC-00-PL-A-1100</b>				Revision <b>1.5</b>	





Scale 1 : 100	Designed SC	Drawn SC	Checked DC	Authorised MC
Original Size A1	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020
Drawing Number SJL-CC-01-PL-A-1101				Revision 1.2





Level 2 - Apartment Schedule			
Level	Type	Family	Count
Level 2	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	5
Level 2	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	4
Level 2	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1
Level 2	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1

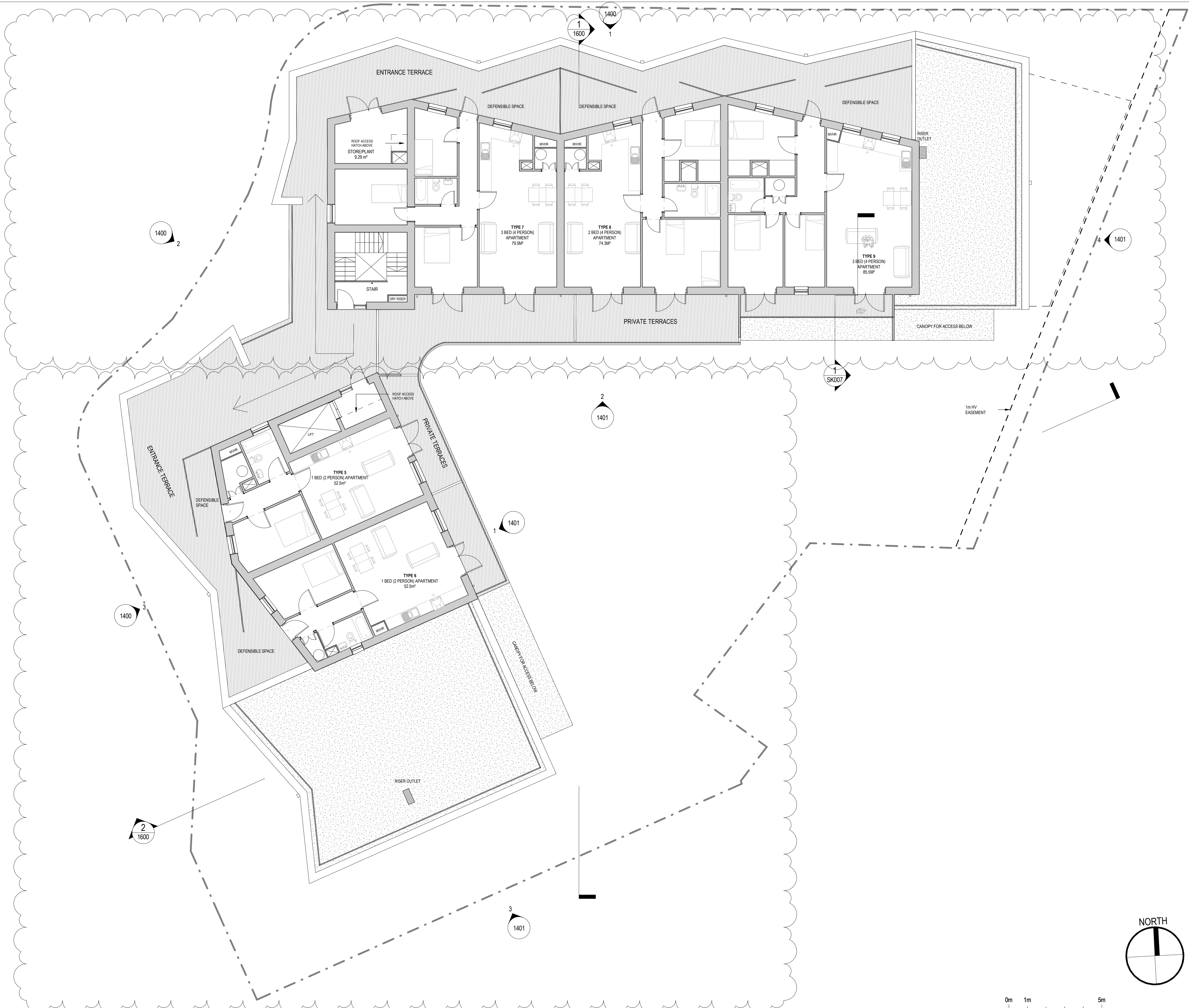
Level 2: 11  
Grand total: 11

Drawing Status	<b>PLANNING</b>	Suitability <b>SO</b>
----------------	-----------------	--------------------------



Scale 1 : 100	Designed SC	Drawn SC	Checked DC	Authorised MC
Original Size A1	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020
Drawing Number SJL-CC-02-PL-A-1102				Revision 1.2





It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement

Notes :

- Drawings are shown for design intent only and subject to further development.
- All dimensions are in millimetres unless otherwise specified.
- The level of information in this drawing is issued for information only. The information shown should not be used for any other purpose (such as building regulations, costing or construction) unless permission is expressly given by client/architect. To be read in conjunction with Civil, Structural and MEP information. Rights for party wall and rights of light lies with the client. A specialist measured survey has not been carried out on the site. All dimensions should be checked out on site. Illustrated material subject to copyright.

Master Apartment Schedule				
Level	Type	Family	Count	
Level 0	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	4	
Level 0	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	3	
Level 0	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 0	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 0: 9				
Level 1	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	5	
Level 1	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	4	
Level 1	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 1	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 1: 11				
Level 2	TYPE 1	1 BED (2 PERSON) APARTMENT - 52.4m² GIA	5	
Level 2	TYPE 2	2 BED (4 PERSON) APARTMENT - 72.5m² GIA	4	
Level 2	TYPE 3	3 BED (5 PERSON) APARTMENT - 96.8m² GIA	1	
Level 2	TYPE 4	3 BED (6 PERSON) APARTMENT - 108.8m² GIA	1	
Level 2: 11				
Level 3	TYPE 5	1 BED (2 PERSON) APARTMENT - 52.5m² GIA	1	
Level 3	TYPE 6	1 BED (2 PERSON) APARTMENT - 52.5m² GIA	1	
Level 3	TYPE 7	3 BED (4 PERSON) APARTMENT - 79.5m² GIA	1	
Level 3	TYPE 8	2 BED (4 PERSON) APARTMENT - 74.3m² GIA	1	
Level 3	TYPE 9	3 BED (4 PERSON) APARTMENT - 85.5m² GIA	1	
Level 3: 5				
Grand total: 36				

Level 3 - Apartment Schedule				
Level	Type	Family	Count	
Level 3	TYPE 5	1 BED (2 PERSON) APARTMENT - 52.5m² GIA	1	
Level 3	TYPE 6	1 BED (2 PERSON) APARTMENT - 52.5m² GIA	1	
Level 3	TYPE 7	3 BED (4 PERSON) APARTMENT - 79.5m² GIA	1	
Level 3	TYPE 8	2 BED (4 PERSON) APARTMENT - 74.3m² GIA	1	
Level 3	TYPE 9	3 BED (4 PERSON) APARTMENT - 85.5m² GIA	1	
Level 3: 5				
Grand total: 5				

1.3	09-04-2021	RED LINE BOUNDARY, BUILDING FOOTPRINT, MASSING AND APARTMENT AMENDMENTS	SC	DC	MC
1.2	04-02-2021	WIP CLIENT REVIEW	SC		
1.1	29-01-2021	WIP CLIENT REVIEW	SC	DC	MC
1.0	30-09-2020	PLANNING	SC	DC	MC
Rev.	Date	Description	By	Chk'd	App'd

Drawing Status

PLANNING

Subsidiary


S0

Cryer&Coe architects

10 - 12 Gloucester Rd,  
Third Floor,  
Bristol,  
BS7 8AE

Tel: 01173 634033  
hello@cryerandcoe.co.uk  
www.cryerandcoe.co.uk

Client

  
more than just a roof

Project Title

ST JOHNS LANE  
BEDMINSTER, BRISTOL, BS3 5AS

Drawing Title

LEVEL 3 - GENERAL ARRANGEMENT PLAN  
PROPOSED

Scale	Designed	Drawn	Checked	Authorised
1:100	SC	SC	DC	MC
Original Size	Date	Date	Date	Date
A1	30-09-2020	30-09-2020	30-09-2020	30-09-2020
Drawing Number	Revision			
SJL-CC-03-PL-A-1103				1.3



### 3 PRINCIPAL GENERAL ARRANGEMENT WEST LEVATION WEDMORE VALE

It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement

Notes :

1. Drawings to be shown in conjunction with all other disciplines drawings and specification documents.
2. Drawings are shown to design intent only and subject to further development.
3. All dimensions are in millimetres unless otherwise specified.
4. The level of information in this drawing is issued for information only. The information shown should not be used for any other purpose (such as building regulations, costing or construction) unless permission is expressly given by client/architect. To be read in conjunction with Civil, Structural and MEP information. Rights for part wall and rights of light with the client. A specialist measured survey has not been carried out on the site. All dimensions should be checked out on site. Illustrated material subject to copyright.

## MATERIAL LEGEND

- 1 - STRETCHER BOND - MULTI RED STOCK BRICK
- 2 - ALTERNATING PROJECTING COURSE - MULTI RED STOCK BRICK
- 3 - HIT & MISS - MULTI RED STOCK BRICK
- 4 - RENDER - OFF WHITE
- 5 - STANDING SEAM SHEET METAL - LIGHT GREY
- 6 - POWDER COATED METAL WORK - LIGHT GREY (RAL 7044)
- 7 - RAINWATER GOODS - LIGHT GREY
- 8 - SINGLE PLY MEMBRANE - GREY
- 9 - COMPOSITE DECKING
- 10 - FIBRE CEMENT DIVIDING SCREENS - LIGHT GREY (RAL 7044)
- 12 - EXHAUST & SUPPLY AIR DUCTS
- 13 - FULL HEIGHT METAL SCREEN WITH ACCESS GATE (RAL 7044)
- 14 - STODECO FACADE PROFILED RENDER SYSTEM (OFF-WHITE)
- 15 - LOW LEVEL BRICK WALL WITH TIMBER FENCING ABOVE (2M TOTAL HEIGHT)
- 16 - FIBRE CEMENT CLADDING - LIGHT GREY (RAL 7044)
- 17 - SEDUM GREEN ROOF

NOTE: ALL EXTERNAL WINDOW & DOOR FRAMES  
ARE TO BE LIGHT GREY uPVC (RAL 7044)

[illegible]

1.1	09-04-2021	RED LINE BOUNDARY, BUILDING FOOTPRINT, BIN, BIKE AND MASSING AMENDMENTS	SC	DC	MC
1.0	30-09-2020	PLANNING			
Rev.	Date	Description	By	Chk'd	App'd

Drawing Status	<b>PLANNING</b>	Suitability <b>S0</b>
----------------	-----------------	--------------------------

Cryer &amp; Coe architects

10 - 12 Gloucester Rd,  
Third Floor,  
Bristol,  
BS7 8AF

Tel: 01173 634033  
hello@cryerandcoe.co.uk  
www.cryerandcoe.co.uk



Project Title

ST JOHNS LANE  
BEDMINSTER, BRISTOL, BS3 5AS

Drawing Title

GENERAL ARRANGEMENT ELEVATIONS  
PROPOSED

Scale 1 : 100	Designed SC	Drawn SC	Checked DC	Authorised MC
Original Size A1	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020	Date 30-09-2020
Drawing Number SJL-CC-ZZ-EL-A-1400				Revision 1.1



Model Name (ENTER MODEL NAME HERE)  
Model File Location (ENTER MODEL FILE LOCATION HERE) - Model Status (S0) - Model Issue Date (MODEL ISSUE DATE)

86 e6e2

100  
0 10  
Millimetres

NOT PROTECTIVELY MARKED

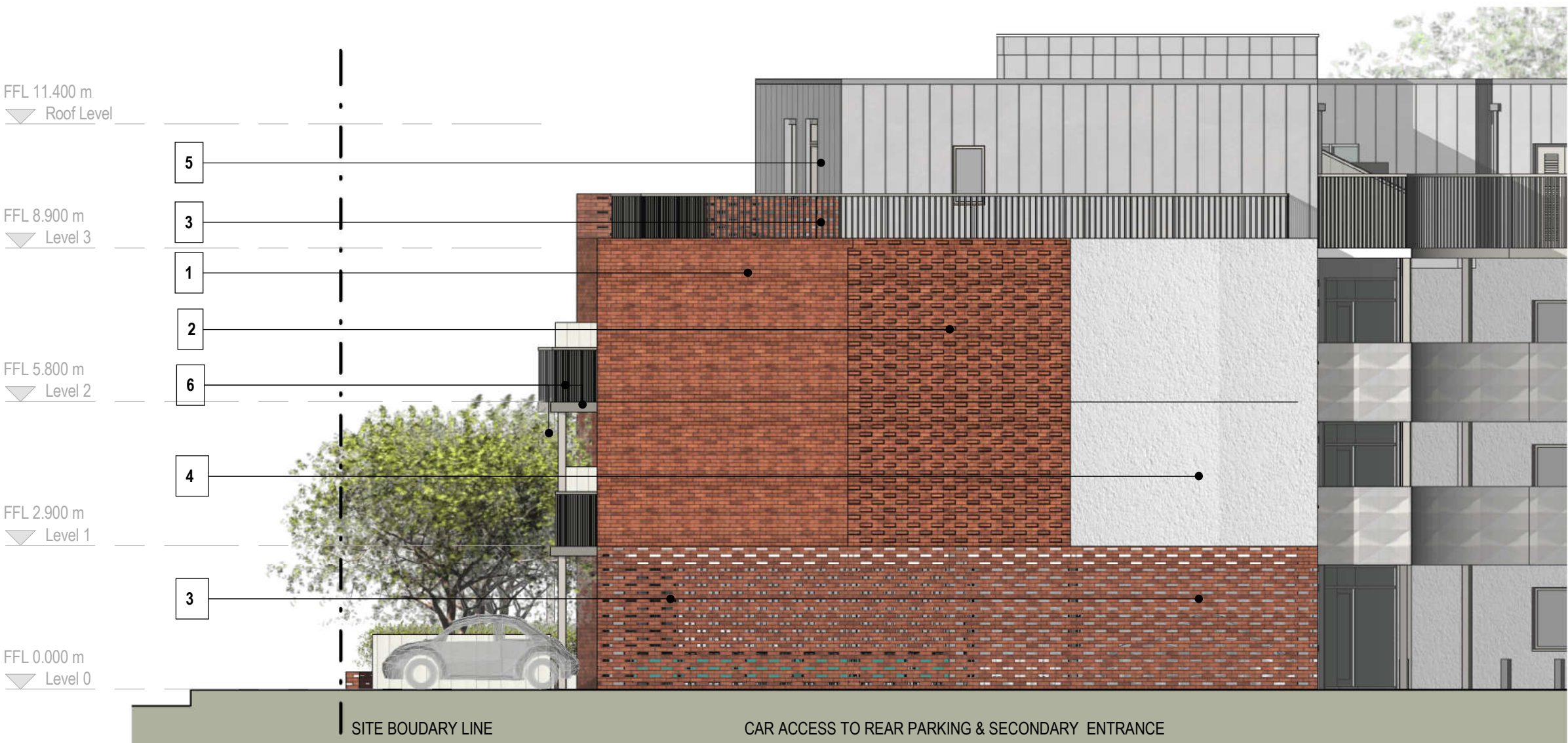
1 GENERAL ARRANGEMENT EAST ELEVATION (FROM CARPARK)  
1401 SCALE 1:100



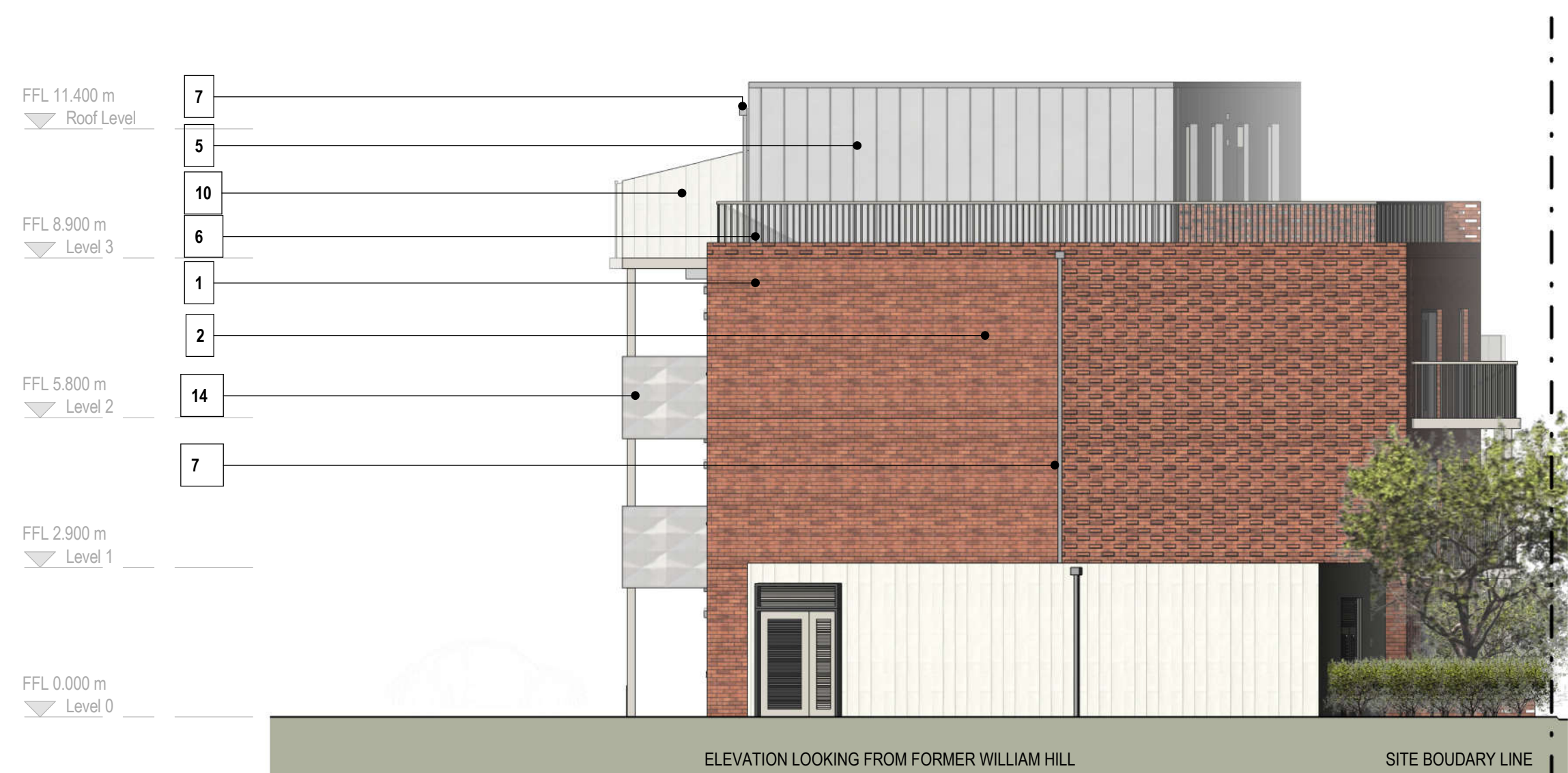
2 GENERAL ARRANGEMENT SOUTH ELEVATION (FROM CARPARK)  
1401 SCALE 1:100



3 GENERAL ARRANGEMENT SOUTH ELEVATION (SIDE)  
1401 SCALE 1:100



4 GENERAL ARRANGEMENT EAST ELEVATION (SIDE)  
1401 SCALE 1:100



NOT PROTECTIVELY MARKED

It is assumed that all works will be carried out by a competent contractor working, where appropriate, to an approved method statement

- Notes :
1. Drawings to be read in conjunction with all other disciplines drawings and specification documents
  2. Drawings are shown for design intent only and subject to further development.
  3. All dimensions are in millimetres unless otherwise specified.
  4. The level of information in this drawing is issued for information only. The information shown should not be used for any other purpose (such as building regulations, costing or construction) unless permission is expressly given by client/architect. To be read in conjunction with Civil, Structural and MEP information. Rights for party wall and rights of light lies with the client. A specialist measured survey has not been carried out on the site. All dimensions should be checked out on site. Illustrated material subject to copyright.

#### MATERIAL LEGEND

- 1 - STRETCHER BOND - MULTI RED STOCK BRICK
- 2 - ALTERNATING PROJECTING COURSE - MULTI RED STOCK BRICK
- 3 - HIT & MISS - MULTI RED STOCK BRICK
- 4 - RENDER - OFF WHITE
- 5 - STANDING SEAM SHEET METAL - LIGHT GREY
- 6 - POWDER COATED METAL WORK - LIGHT GREY (RAL 7044)
- 7 - RAINWATER GOODS - LIGHT GREY
- 8 - SINGLE PLY MEMBRANE - GREY
- 9 - COMPOSITE DECKING
- 10 - FIBRE CEMENT DIVIDING SCREENS - LIGHT GREY (RAL 7044)
- 12 - EXHAUST & SUPPLY AIR DUCTS
- 13 - FULL HEIGHT METAL SCREEN WITH ACCESS GATE (RAL 7044)
- 14 - STODECO FACADE PROFILED RENDER SYSTEM (OFF-WHITE)
- 15 - LOW LEVEL BRICK WALL WITH TIMBER FENCING ABOVE (2M TOTAL HEIGHT)
- 16 - FIBRE CEMENT CLADDING - LIGHT GREY (RAL 7044)
- 17 - SEDUM GREEN ROOF

NOTE: ALL EXTERNAL WINDOW & DOOR FRAMES ARE TO BE LIGHT GREY uPVC (RAL 7044)

SC	09-04-2021	RED LINE BOUNDARY, BUILDING FOOTPRINT, BIN, BIKE, MATERIALITY AND MASSING AMENDMENTS	SC	DC	MC
SC	30-09-2020	PLANNING	SC	DC	MC
Rev.	Date	Description	By	Chk'd	App'd

Drawing Status	PLANNING	Substability	S0
----------------	----------	--------------	----

Cryer&Coe architects

10 - 12 Gloucester Rd,  
Third Floor,  
Bristol,  
BS7 8AE

Tel: 01173 634033  
hello@cryerandcoe.co.uk  
www.cryerandcoe.co.uk



Project Title

ST JOHN'S LANE  
BEDMINSTER, BRISTOL, BS3 5AS

Drawing Title

GENERAL ARRANGEMENT ELEVATIONS  
PROPOSED

Scale	1:100	Designed	SC	Drawn	SC	Checked	DC	Authorised	MC
Original Size	A1	Date	30-09-2020	Date	30-09-2020	Date	30-09-2020	Date	30-09-2020
Drawing Number	SJL-CC-ZZ-EL-A-1401	Revision	1.1						